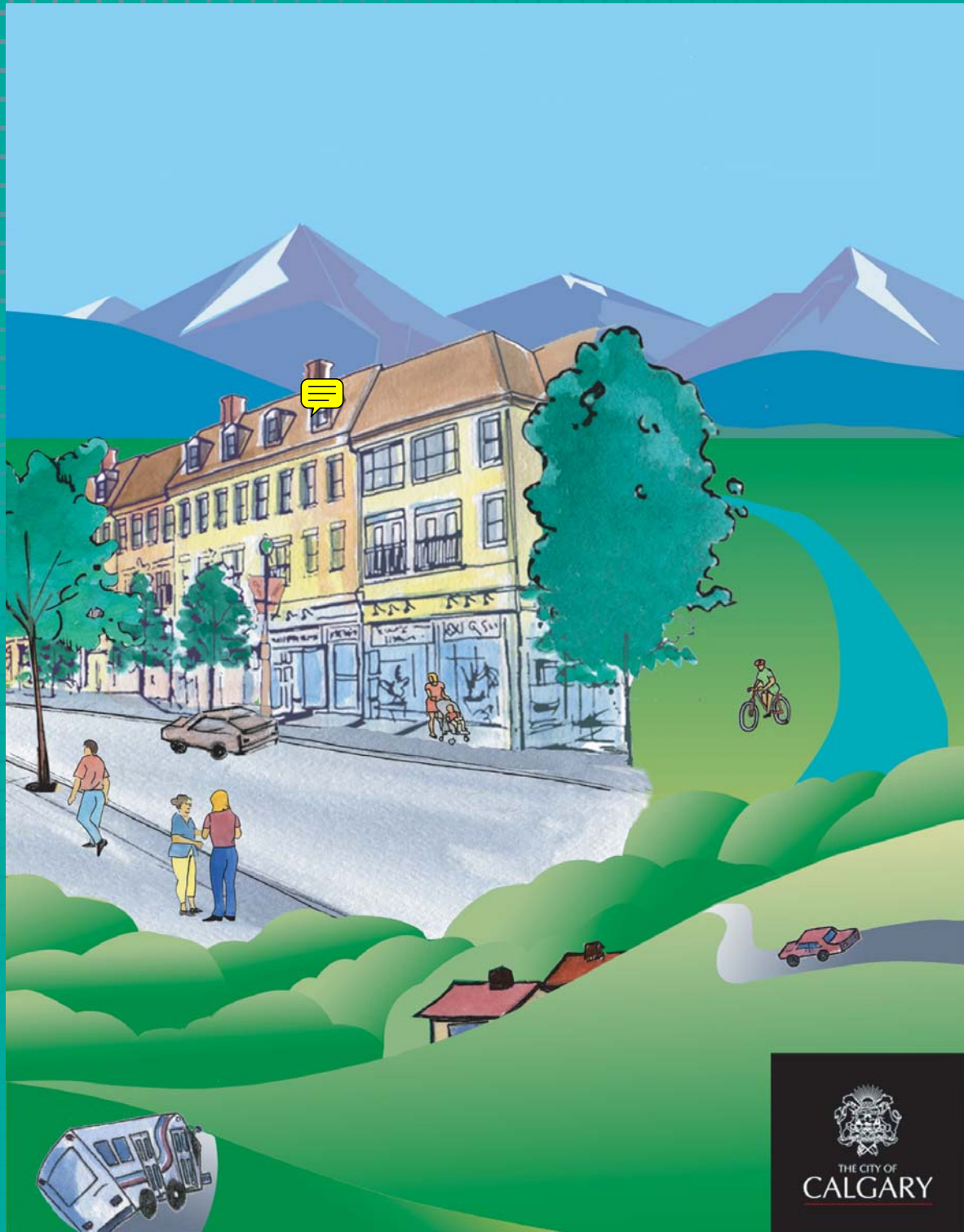


# Montgomery Area Redevelopment Plan





**Office Consolidation**  
**2012 June**

# **MONTGOMERY**

## **AREA REDEVELOPMENT PLAN**

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NOTE: This office consolidation includes the following amending Bylaw:

Amendment	Bylaw	Date	Description
1	19P2008	2008 April 14	(a) Map 1.3 delete and replace. (b) Add new Policy "Service Organization" Policy P10 in Section 4.5, under Park Policies.
2	38P2008	2008 June 01	(a) Delete text in Section 1.3. (b) Add text in Section 1.3. (c) Delete and replace text in Section 1.14. (d) Delete text in Policy R7. (e) Delete text in Policy R8. (f) Delete subheading and Policy R10. (g) Insert text in Policy R18. (h) Delete text from the note below Policy R18. (i) Delete text in Guideline R10(ii). (j) Delete text in Policy C2. (k) Delete and replace text in Policy C13. (l) Delete and replace text in Policy C14. (m) Delete text in Policy C23. (n) Delete text in Section 5.7. (o) Delete and replace text in Policy T3. (p) Delete and replace text in the first bullet action point pertaining to "Bowness Road NW from 51 Street NW to Mackay Road NW" under Table 6.6 Transportation Actions.
3	13P2012	2012 June 11	(a) Map 1.3 delete and replace.

Amended portions of the text are printed in *italics* and the specific amending Bylaw is noted.

Persons making use of this consolidation are reminded that it has no legislative sanction, and that amendments have been embodied for ease of reference only. The official Bylaw and all amendments are available from the City Clerk and should be consulted when interpreting and applying this Bylaw.

## **PUBLISHING INFORMATION**

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# SECTION 1: POLICY

## Chapter 1

## Introduction & Plan Summary



*Montgomery has a range of characteristics that contribute to its small-town character and are reflective of its historic development as a separate municipality (Montgomery was annexed to Calgary in 1963). For example, a traditional grid street pattern (1), well-defined community boundaries (1 & 2), a large stock of mature trees (3) and a high proportion of public open space (1 & 3).*

FIGURE 1.1

## LOCATION PLAN

### Legend

 ARP Boundary

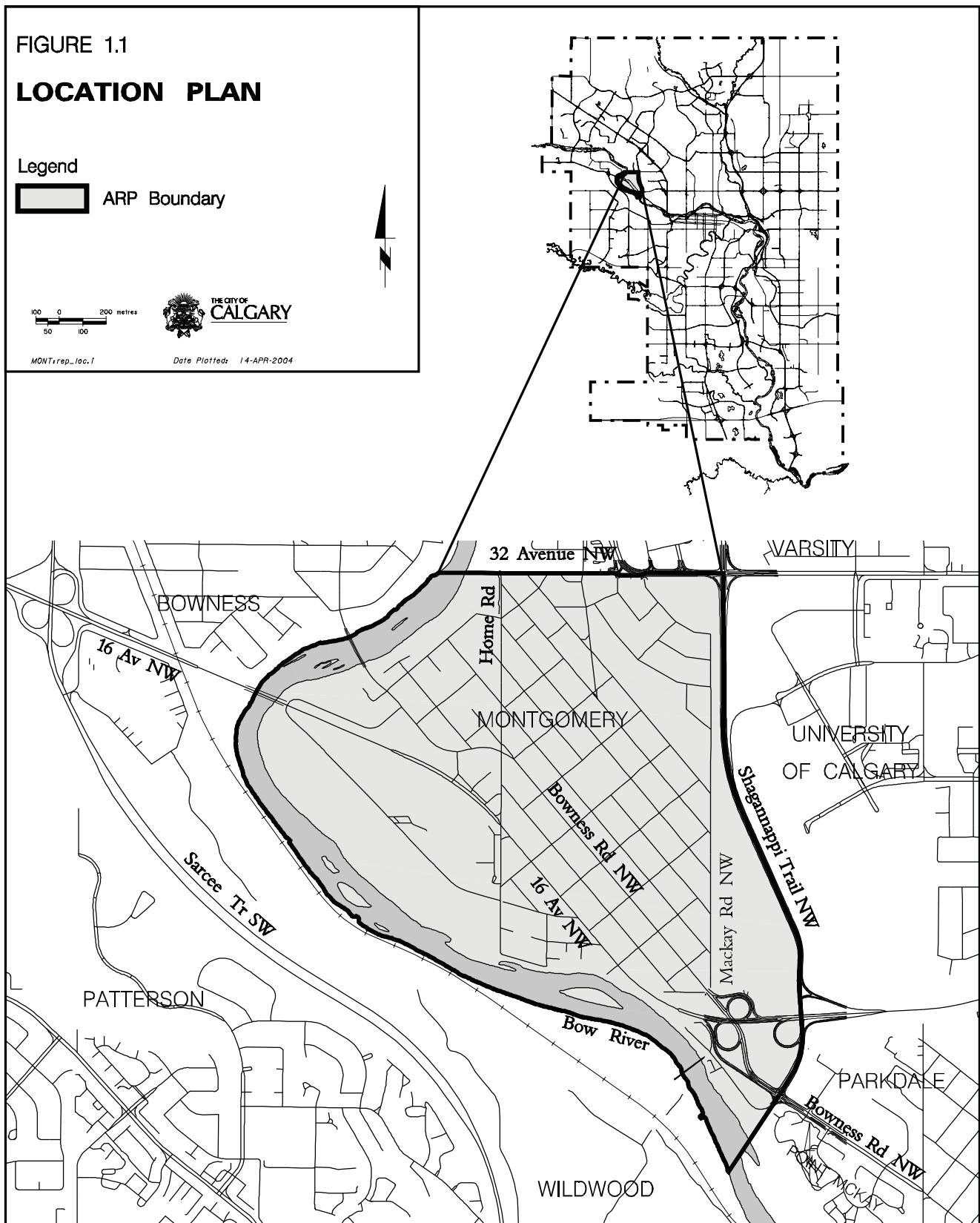
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CALGARY

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## PURPOSE OF THE PLAN

- 1.1 Area Redevelopment Plans (ARPs) are planning documents that set out comprehensive land use policies and other proposals to help guide the future of individual communities. Area Redevelopment Plans are specific to the particular community and must be considered in conjunction with other applicable statutory plans, bylaws and policy documents.
- 1.2 This Area Redevelopment Plan outlines a policy direction relative to land use and physical planning matters as identified in Sections 634 and 635 of the *Municipal Government Act*. The purpose of this ARP is to establish land use and development policies and provide guidance for City Administration in undertaking actions and programs.
- 1.3 The Montgomery ARP supplements *The Calgary Land Use Bylaw* by providing a local policy context and specific land use and development guidelines on which the Approving Authority can base its decision. When rendering decisions the Approving Authority must consider the contents of both the Montgomery ARP and *The Calgary Land Use Bylaw*. **Bylaw 38P2008**

*Note: This ARP was adopted by Council when the City of Calgary Land Use Bylaw 2P80 ("2P80") was in effect. As a result, the ARP references land use districts both in its text and its maps which are no longer current. New land use districts have been applied to all parcels in the City, pursuant to the City of Calgary Land Use Bylaw 1P2007 ("1P2007"), effective June 1, 2008, which transitioned 2P80 districts to the most similar 1P2007 district. Therefore, it is important for the user of this ARP to consult the new land use maps associated with 1P2007 to determine what the actual land use designation of a general area or specific site would be. Any development permit applications will be processed pursuant to the districts and development rules set out in 1P2007.*

*Notwithstanding the foregoing, the user should be aware that where the ARP guidelines and policies reference a 2P80 district in the ARP, the same guidelines and policies will be applicable to those lands identified by the district on an ongoing basis and must be considered by the approving authority in its decision making, notwithstanding that the 2P80 districts, strictly speaking have no further force and effect.* **Bylaw 38P2008**

## PLANNING HORIZON

- 1.4 The planning timeframe for the Montgomery ARP is anticipated to be 10-15 years. This planning period may vary in relation to the general growth trends within the city or to specific trends in Montgomery. It is important therefore, that an evaluation of the effectiveness of the ARP in meeting its objectives be undertaken when circumstances warrant.

## PLAN FORMAT

- 1.5 The Montgomery ARP is made up of three sections, Policy, Action and Background.
- 1.6 The City of Calgary Council adopts the **Policy Section** as a bylaw. Changes to the policies and/or maps will require an amendment to the bylaw with a public hearing and advertising requirements as set out in the *Municipal Government Act*.
- 1.7 The **Action Section** contains a series of recommendations relevant to various City Departments and the Community Association. This section also includes the land use redesignations resulting from this Plan. This section is not part of the ARP Bylaw.
- 1.8 The **Background Section** contains supporting information, relevant research and the community perspective and is not part of the ARP Bylaw.

## PLAN BOUNDARIES

- 1.9 The community of Montgomery is located on the north side of the Bow River in northwest quadrant of Calgary (see Figures 1.1 & 1.2). Well-defined edges serve as community boundaries. To the west and south, Montgomery is bounded by the Bow River. The easterly boundary is Shaganappi Trail NW and the northerly boundary is 32nd Avenue NW. The community's edge conditions help define Montgomery as a distinct community with its own character.

## AVAILABILITY OF MUNICIPAL FUNDS FOR IMPROVEMENT PROJECTS

- 1.10 Municipal public facilities and improvements proposed in this Community Plan are subject to the City's capital budget priorities and approval process. Programs recommended in this Plan will be evaluated in relation to the needs of other communities and city-wide spending priorities.

## PLAN IN SUMMARY

- 1.11 The future principle land uses envisaged by the ARP are detailed in the Land Use Plan (Figure 1.3). The ARP's strategy has 11 major components as set out below (also refer to Figure 1.2).

### 1. **Revitalize the Bowness Road/46 Street Commercial Area as a Mixed Use Core**

- 1.12 The ARP recognizes the Bowness Road/46 Street Commercial Area as having strategic importance for the future image of the community. Policies and design guidelines are proposed with the objective of encouraging a transition to a pedestrian friendly mixed use (commercial/residential) area.

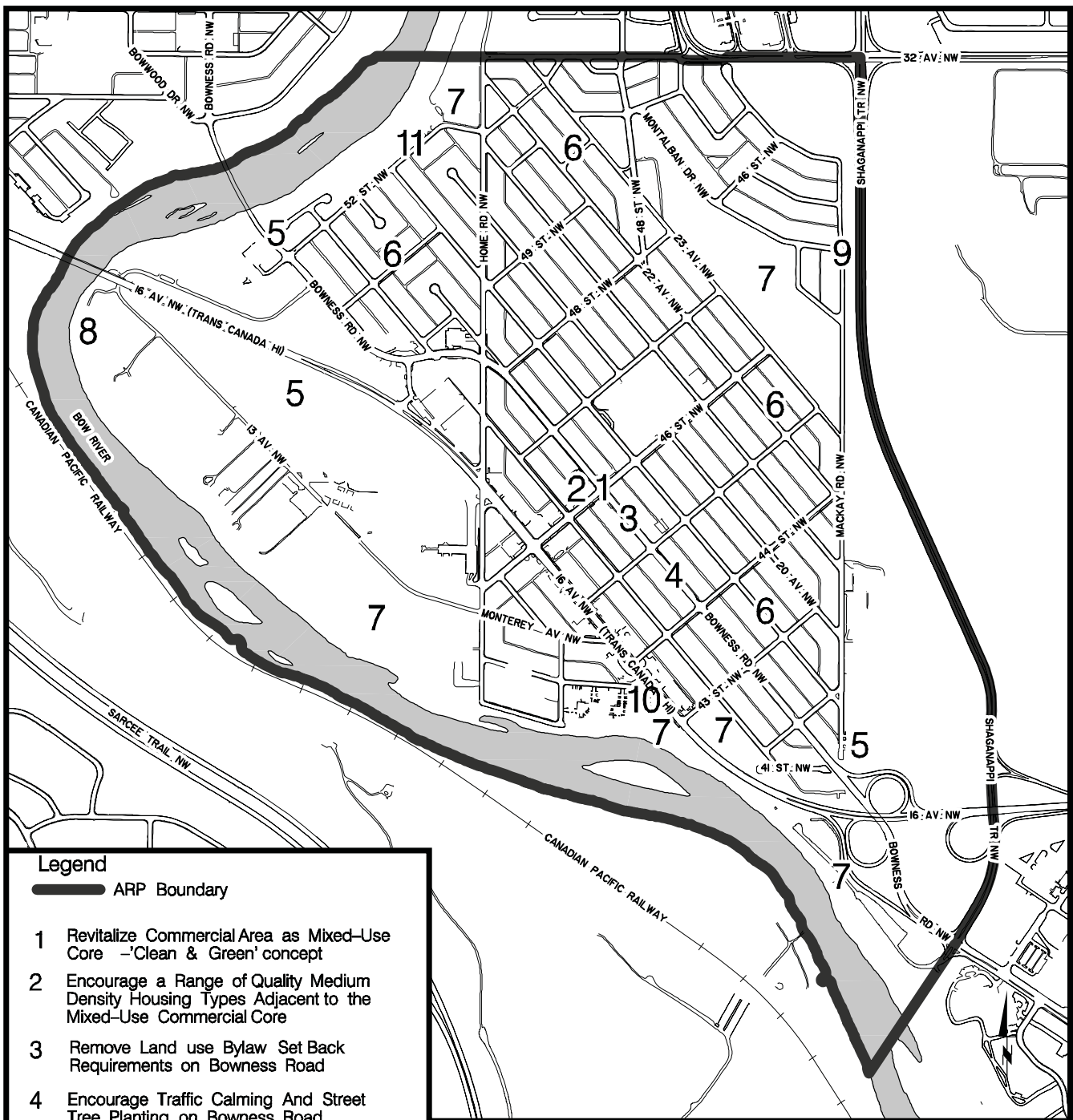


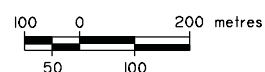
FIGURE 1.2

### Legend

**ARP Boundary**

- 1 Revitalize Commercial Area as Mixed-Use Core - "Clean & Green" concept
- 2 Encourage a Range of Quality Medium Density Housing Types Adjacent to the Mixed-Use Commercial Core
- 3 Remove Land use Bylaw Set Back Requirements on Bowness Road
- 4 Encourage Traffic Calming And Street Tree Planting on Bowness Road
- 5 Encourage Attractive Community Gateways
- 6 Ensure Quality Low Density Residential Redevelopment, and, where appropriate, Slope Adaptive design
- 7 Maintain and Enhance Existing Parks
- 8 Protect Floodplain/Floodway for Passive Recreation
- 9 Pedestrian Overpass to be Determined
- 10 Future Pedestrian Overpass Location
- 11 Encourage Two-Way Bike and Pedestrian Route

### ARP IN SUMMARY



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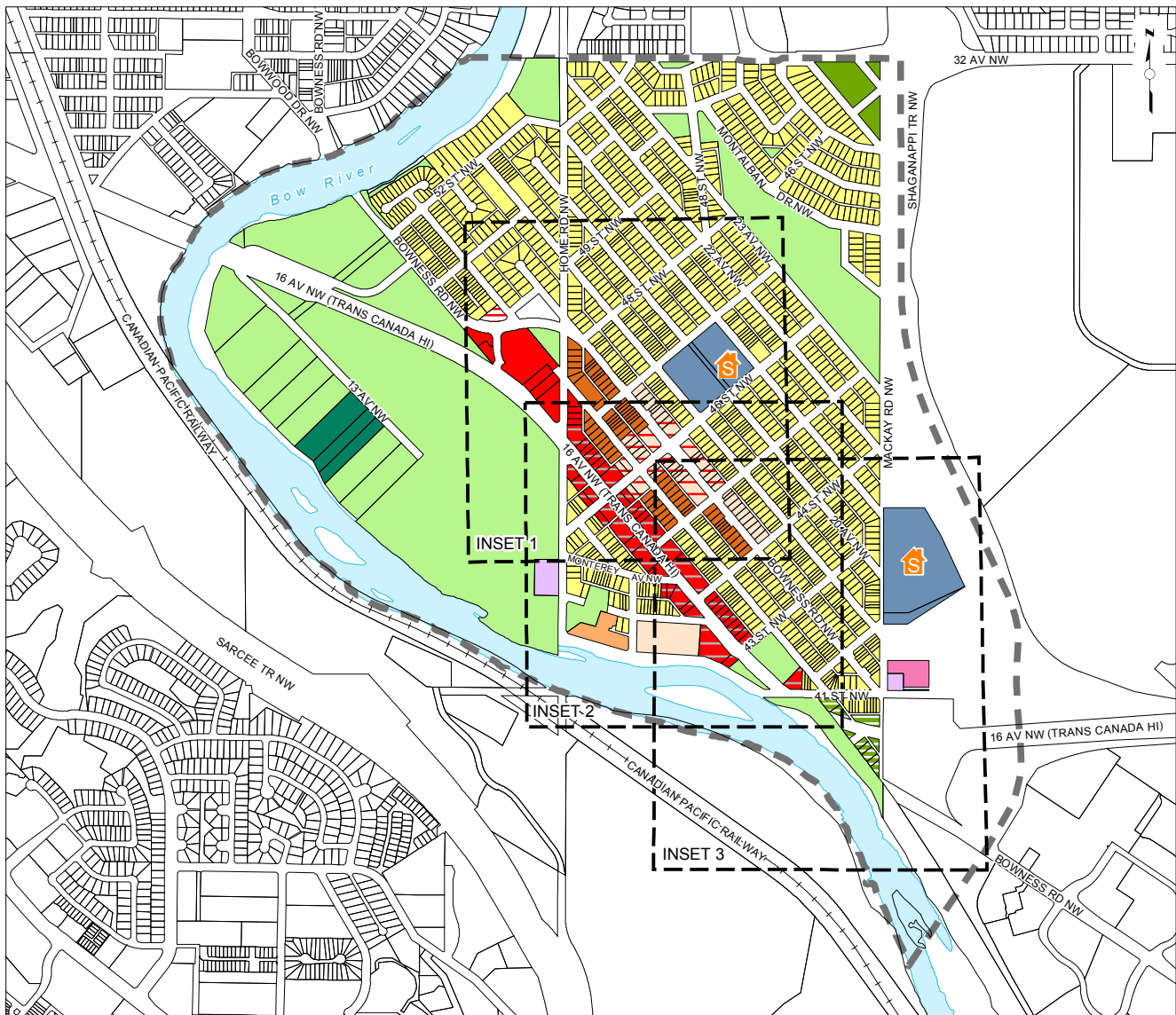
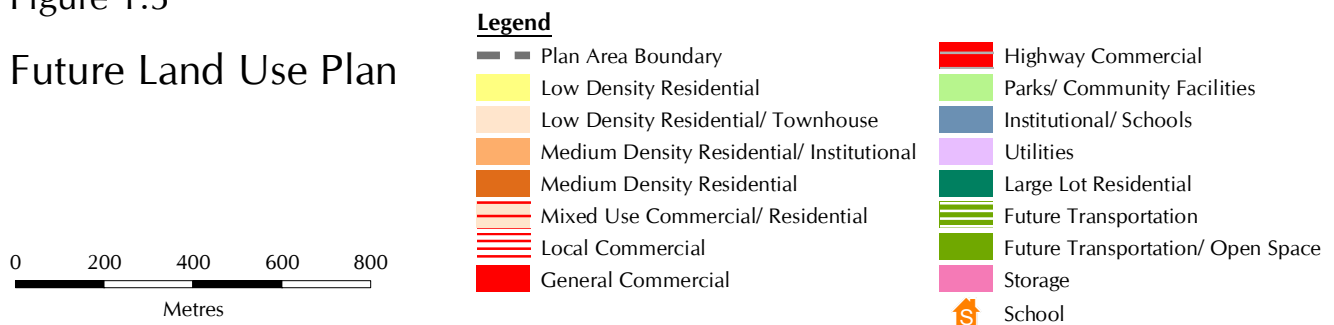


Figure 1.3

## Future Land Use Plan



Approved: 11P2004  
Amended: 19P2008, 13P2012

This map is conceptual only. No measurements of distances or areas should be taken from this map.

Insets 1-3 are provided in larger scale in the background information section for ease of reference

## **2. Encourage a Range of Quality Medium Density Housing Types near to the Mixed-Use Commercial Core**

- 1.13 The ARP encourages a sensitive increase in residential density near the Bowness Road/46 Street Commercial Core. This would extend the range of housing types available in the community and increase the local customer base for nearby businesses. Policies and design guidelines are proposed to ensure new buildings are pedestrian orientated and integrate well with nearby low density residential areas.

## **3. Remove Land Use Bylaw Setback Requirements on Bowness Road**

- 1.14 Existing property setback requirements established in *the Land Use Bylaw* require a 5.182 metre setback on both the north and south sides of Bowness Road. These setbacks do not support the long term vision for the enhancement of the commercial area. Design work completed as part of the ARP process demonstrates that an enhanced pedestrian environment could be accommodated within the existing right-of-way for Bowness Road.
- Bylaw 38P2008**

## **4. Encourage Traffic Calming and Tree Planting on Bowness Road**

- 1.15 The Plan supports streetscape enhancements to Bowness Road for the purposes of area revitalization and traffic calming. An urban design concept based on a “Clean and Green” theme was prepared in conjunction with this Plan to provide a framework for redevelopment.

## **5. Encourage Attractive Community Gateways**

- 1.16 The Plan encourages tree planting to enhance gateways into Montgomery. Gateway priorities include lands adjacent to the Trans Canada Highway and Bowness Road. The Community Association is encouraged to install community identification signs at all entry points.

**6. Ensure Quality Low Density Residential Development and, Where Appropriate, Slope Adaptive Design**

- 1.17 Policies and design guidelines are provided in the Plan aimed at ensuring high quality residential development that enhances Montgomery's image. Topography is recognized by ensuring development in areas of significant slopes ( $\geq 15\%/8.5^\circ$ ) consider slope adaptive design and environmental impacts.

**7. Maintaining & Enhancing Existing Parks**

- 1.18 The ARP recommends a number of policies and local improvement initiatives with the objectives of retaining and enhancing existing parks and encouraging their use for a variety of recreational leisure and cultural activities.

**8. Protect Floodplain/Floodway Lands for Passive Recreation**

- 1.19 The ARP promotes the use of floodplain and floodway land for passive recreational use.

**9. & 10. Pedestrian Overpasses**

- 1.20 The construction of a pedestrian overpass over 16 Avenue and at 43 Street is one of the City's top priorities. A pedestrian overpass of Shaganappi Trail linking Montgomery to University of Calgary Lands is proposed in The Calgary Pathways and Bikeways Implementation Plan and is supported by this Plan.

**11. Encourage Two-Way Bike and Pedestrian Pathway or Curb Lane**

- 1.21 The creation of a river pathway link on the east side of the Bow River behind the dwellings on 52 Street NW is currently not feasible due to topographical and land ownership constraints. 52 Street NW is currently designated as a one way on-street bicycle route. The development of a two way bike and pedestrian route at this location is supported by the Plan.

## Chapter 2

## Residential Land Use



*The ARP promotes low-profile, medium-density development near Montgomery's existing commercial core on Bowness Road. For example, three-storey apartments (1), townhouses (2 & 4), and fourplex residential units (3).*

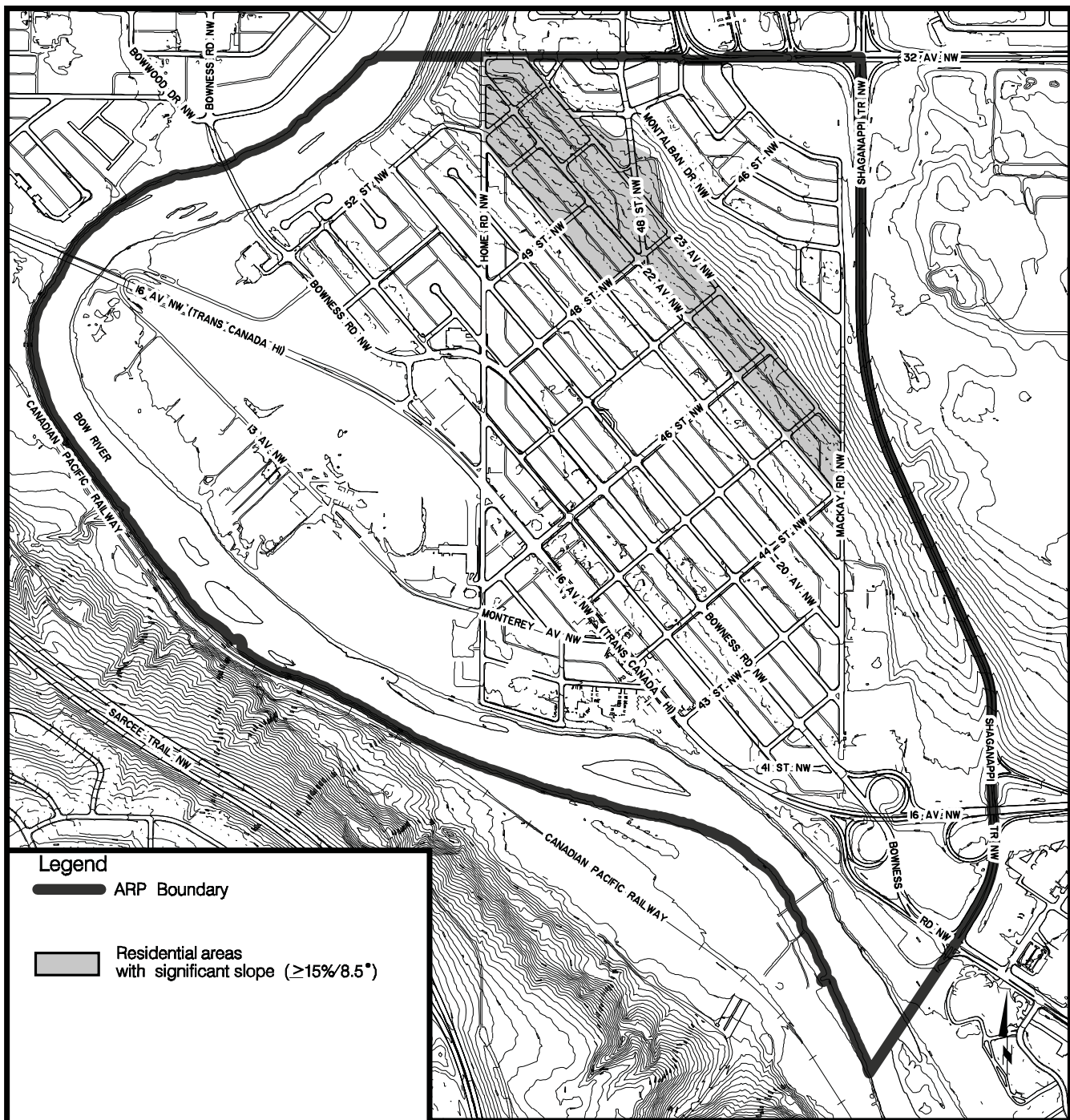
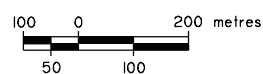


FIGURE 2.1

## RESIDENTIAL AREAS WITH SIGNIFICANT SLOPE



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## BACKGROUND

- 2.1 Montgomery is predominately a low-density residential community with 47% of its gross developable area (77 ha, 190 acres) currently designated for lower density residential development (R-1 and R-2). A small percentage (4%) of the gross developable area is currently designated for a combination of townhouse style development (R-2A) and for medium density residential development.
- 2.2 From 1968–2001, Montgomery’s population declined by 26% and during the same period the number of dwelling units increased by 28%. These trends are similar to other inner city communities where smaller household sizes have resulted in population decline in spite of an increase in the number of dwelling units. In addition to population decline, Montgomery’s population is aging. In the year 2001, 17% of Montgomery’s population were over 65, compared to 9% for Calgary as a whole.
- 2.3 Section 3 Background Information provides more detailed information regarding residential land use in Montgomery.

## RESIDENTIAL VISION FOR THE FUTURE

- 2.4 *Montgomery continues to be a predominately low-density residential community with a small-town character. However, some sensitive intensification has occurred in the form of good-quality medium-density residential development located near to the Bowness Road/46 Street Commercial Core. This development extends the range of housing types available in the community, as well as increasing the local customer base for nearby businesses. New buildings are pedestrian-oriented and integrate well with nearby low-density residential areas. In the areas of significant slope, new houses have a building form which steps down to follow the existing land contours.*

## RESIDENTIAL OBJECTIVES

- 2.5
- (a) To preserve Montgomery’s small town character by ensuring residential development contributes to preserving its predominantly low-density land use.
  - (b) To ensure high quality residential redevelopment that enhances Montgomery’s image.
  - (c) To recognize that Montgomery could accommodate a range of housing design styles, of which primary considerations are high quality design and attention to detail.

- (d) To encourage medium-density residential development that is street-friendly, grade-oriented and designed to integrate well visually and functionally with the pedestrian-street frontage.
- (e) To ensure residential development is not visually dominating and set apart from the street.
- (f) To enhance Montgomery as a sustainable and livable residential community by encouraging a variety of housing types that can accommodate a range of ages, household sizes, household types and incomes.
- (g) To ensure that new development provides an attractive pedestrian-friendly residential environment with adequate parking, landscaping and amenities.
- (h) To encourage soft/natural landscaping to complement Montgomery's river valley and escarpment natural features.
- (i) To recognize the importance of Montgomery's topography by ensuring development in areas of significant slope by considering slope adaptive design and environmental impacts.

## **LOW-DENSITY RESIDENTIAL POLICIES**

### **Land Use**

**Policy R1.** Low-density residential development is encouraged in the areas indicated on the Future Land Use Plan (Figure 1.3). In these locations, all R-1, R-2 and Direct Control Districts with low-density residential guidelines shall continue.

### **Redesignation**

**Policy R2.** The redesignation of low-density residential land (R-1 and R-2 and Direct Control Districts with low-density residential guidelines) to R-2A Residential Low Density District and to medium density residential (RM-4) should only be permitted in the locations indicated on the Land Use Plan (Figure 1.3). Redesignations outside these will require an amendment to the ARP.

**Policy R3.** Redesignation of R-2 Residential Low Density District to R-1 Residential Single Detached District should not be permitted. The R-2 land use designation provides flexibility and a low-density method for increasing and stabilizing Montgomery's population.

- Policy R4.** In order to secure the stability of single detached neighbourhoods and retain community character the redesignation of R-1 Residential Single Detached District to R-2 Low Density Residential District should not be permitted.
- Policy R5.** The redesignation of individual lots from R-1 Residential Single Detached District to R-2 Residential Low Density District is not supported.
- Policy R6.** The redesignation of existing R-1 Residential Single Detached District areas will require a comprehensive amendment to the Plan.

## **Front Yard Setbacks**

- Policy R7.** Montgomery is characterized by greater front yard setbacks than the minimum established in the *Land Use Bylaw*. In order to maintain the established street pattern and limit the visual impact of new development, the front yard setback should generally be consistent with the existing street pattern with the block.
- Bylaw 38P2008**

## **Lot Coverage**

- Policy R8.** In order to avoid out of scale structures on 15 m (50 ft.) wide lots, relaxations to lot coverage as stated in *Land Use Bylaw* should not be granted.
- Bylaw 38P2008**

## **Subdivision Design**

- Policy R9.** The historic pattern of subdivision should be respected. Subdivisions should be oriented to the Avenue. Panhandle subdivisions, reverse corner lots and irregular lot shapes are discouraged.

*Deleted*

**Bylaw 38P2008**

## **52 Street NW**

(Policies only apply to the northern side of 52 Street NW)

- Policy R11.** Minimum lot widths should be set at 15 m (50 ft) for the entire length of the site.
- Policy R12.** 2111 52 Street NW should not be subdivided into any more than three lots and the pattern of subdivision should reflect those of the lots to the west on the northern side of 52 Street NW.

**Policy R13.** The principle building should be set back a minimum of 8 metres from the left bank of the Bow River (as indicated on Plan 7546FN (1946 Subdivision Plan). Where erosion has occurred the eight m setback shall be measured from the crest of the riverbank. No projection or accessory structure should be allowed within the setback area unless it can be demonstrated that the views of adjacent properties would not be impaired or obstructed. The 'Building Depth 60%/40% principle' set out in the Section 4.4 of the *'Infill Housing Guidelines'* do not apply.

**Policy R14.** Minimum side yard widths should be 1.5 metres from the property line.

**Policy R15.** New development within 2111 - 52 Street NW is encouraged to have a side yard width of at least three metres from the western property line to take account of the location of the existing building within 2023 52 Street NW.

### **Office Conversions**

**Policy R16.** Office conversions in areas identified for low density residential on Figure 1.3 should not be permitted as this would alter the residential character of the street.

### **13 Avenue NW**

**Policy R17.** In order to be consistent with the *Urban Park Master Plan* (1994), the redesignation or subdivision of lands identified as large lot residential on the Future Land Use Plan (Figure 1.3) is not supported except for the purpose of expanding Shouldice Park.

### **Low Density Residential & Slope Adaptive Design Guidelines**

**Policy R18.** In areas identified on the Future Land Use Plan (Figure 1.3) as low density residential and Residential Areas with Significant Slopes (Figure 2.1), the following Design Guidelines shall be considered *when reviewing discretionary use development permit applications for residential development*. These design guidelines are supplementary to the *Low Density Housing Guidelines for Established Communities* and are not intended to restrict design flexibility or creativity but to ensure that design diversity is achieved in a manner consistent with the community vision and the plans objectives and policies.

**Bylaw 38P2008**

## NOTE

**The following low density residential and slope adaptive design guidelines can only be applied to new development and additions or renovations which require a development permit.**

*Deleted*

**Bylaw 38P2008**

**Historically, only a third of new additions in Montgomery have required a development permit. Refer to B27 to B29 for further information.**

## LOW DENSITY RESIDENTIAL & SLOPE ADAPTIVE DESIGN GUIDELINES

Guideline R1. New development or renovations/additions should demonstrate attention to detail and a high standard of façade treatment and landscaping as evident in the best examples of infill residential development within inner city areas and established communities.

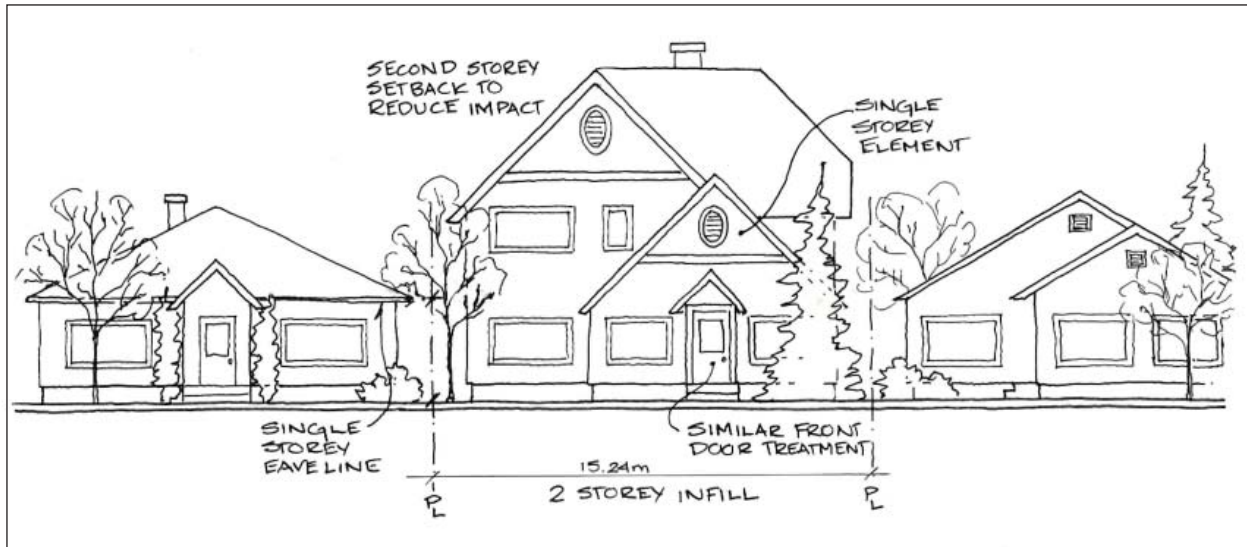


*Excellent facade and landscaping treatment (refer to Guideline R1).*

Guideline R2. It is recognized that single story and small footprint bungalows exemplify much of the original and existing housing stock in Montgomery. A range of architectural styles, from traditional to modern, are considered appropriate provided the development demonstrates attention to detail and is high quality.

Guideline R3. Two storey development should demonstrate sensitivity to the existing character of the community for example by:

- (i) softening the contrast between the existing and new development by using similar eavelines and single storey elements.



- (ii) using contemporary design solutions that demonstrate high quality design and attention to detail.
- (iii) reducing the perception of second storey wall heights through good site and building design, including consideration of roof massing and eaves. Trees may soften the image of two storey structures in the longer term.



*Building mass reduced through roof and eaves variations.*

Guideline R4. Design variations in single detached dwellings are encouraged along the same street as opposed to repeating the same design within the same block length. Mirror image infill is not acceptable.

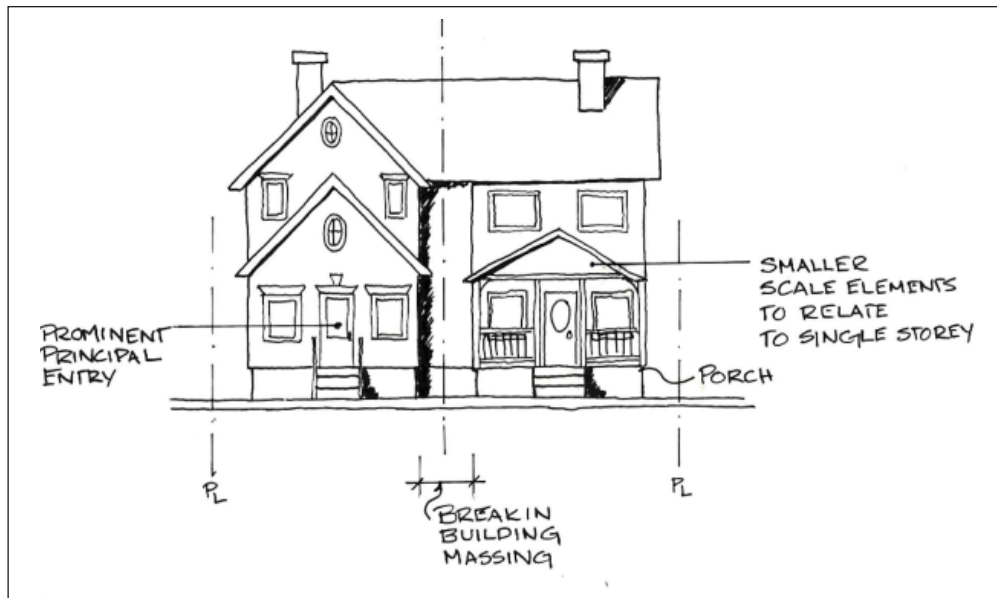
Guideline R5. Facade variations in the roofline form, window form, porch details, door placement, building materials and colour are encouraged.



*Good design variation achieved by differences in the roofline form, window forms, porch detailing, building materials and colour.*

Guideline R6. Semi-detached developments should respond to the development pattern on the street by:

- (i) breaking the building massing to make it more responsive to smaller scaled adjacent buildings;
- (ii) avoiding large scale unified forms spanning the entire building;
- (iii) ensuring the principal entry of each semi-detached unit is clearly visible and identifiable from the street or avenue.



*Note: For illustration only.*

Guideline R7. Corner site development should reflect its dual frontage by presenting an attractive façade to both the frontage and flanking roads.



*Good facade detailing on a corner site.*

Guideline R8. Design variations in semi detached dwellings are encouraged along the same street as opposed to repeating the same design within the same block length. Mirror imaging will only be acceptable if high quality design and attention to detail can be demonstrated.

Guideline R9. Trees are an important component of Montgomery’s residential character and make a vital contribution to the streetscape. Development should maintain mature trees or provide replacement consistent with the *Low Density Housing Guidelines for Established Communities Appendix VIII*.

Guideline R10. New front garages should only be permitted where at least one of the following conditions exist:

- (i) rear lane access is not available;
- (ii) at least 50% of the block face already has front garages; or **Bylaw 38P2008**
- (iii) within residential areas with significant slope as identified in Figure 2.1.

Guideline R11. Front yards should be used as landscaped areas and not for vehicular purposes such as parking or passenger drop off areas. Where a development permit is required for an addition or redevelopment of an existing residential building, existing driveways may be retained provided that:

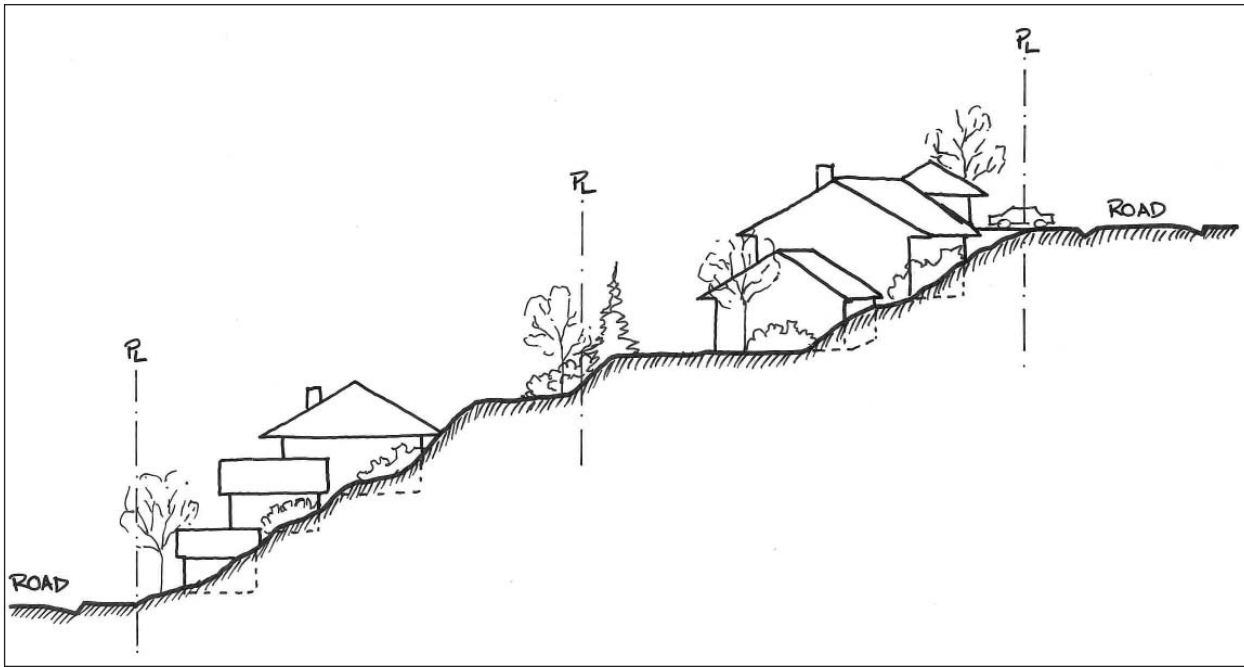
- (i) the subject lot has a minimum frontage of 10.7 metres; and
- (ii) at least 60% of the block face already has front driveways.

Guideline R12. In no case shall a driveway that accommodates a single vehicle be expanded to accommodate two or more vehicles. In situations where the Approving Authority has authorized the retention of an existing front driveway, a garage that does not extend beyond the front facade of the house may be incorporated into the design for the redeveloped lot.

## **Slope Adaptive Residential Design Guidelines**

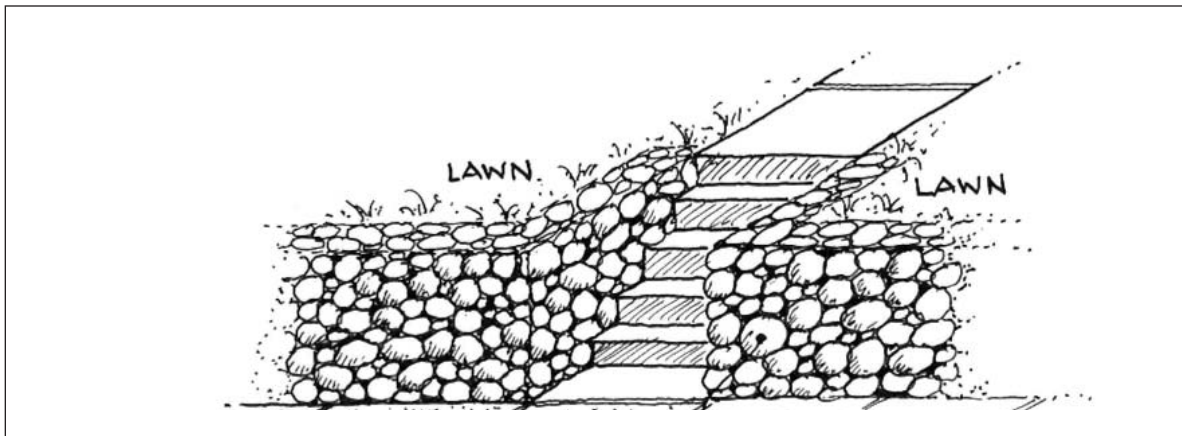
Guideline R13. In existing residential areas identified in Figure 2.1 as having significant slopes ( $\geq 15\%/8.5^\circ$ ) development should demonstrate consistency with following principles:

- (i) building form should be stepped down in order to follow the slope and conform to existing land contours;
- (ii) no unfinished concrete foundation should be exposed above grade at any point;
- (iii) where the rear elevation of a dwelling is highly visible, consideration should be given to its appearance, including landscaping to soften visual impact.



*Buildings should be slope adaptive to achieve a building mass more sympathetic to its environment.*

Guideline R14. Where a new retaining wall is required or where an existing retaining wall must be rebuilt, the use of river boulders, sandstone and other natural and local materials are encouraged as a finish material. Unfinished concrete walls should be avoided. Exposed rip-rap is an unacceptable material for retaining walls.



*The use of river boulders, sandstone and other natural materials in the construction of retaining walls is encouraged.*

## MEDIUM DENSITY RESIDENTIAL POLICIES

### Height/Density

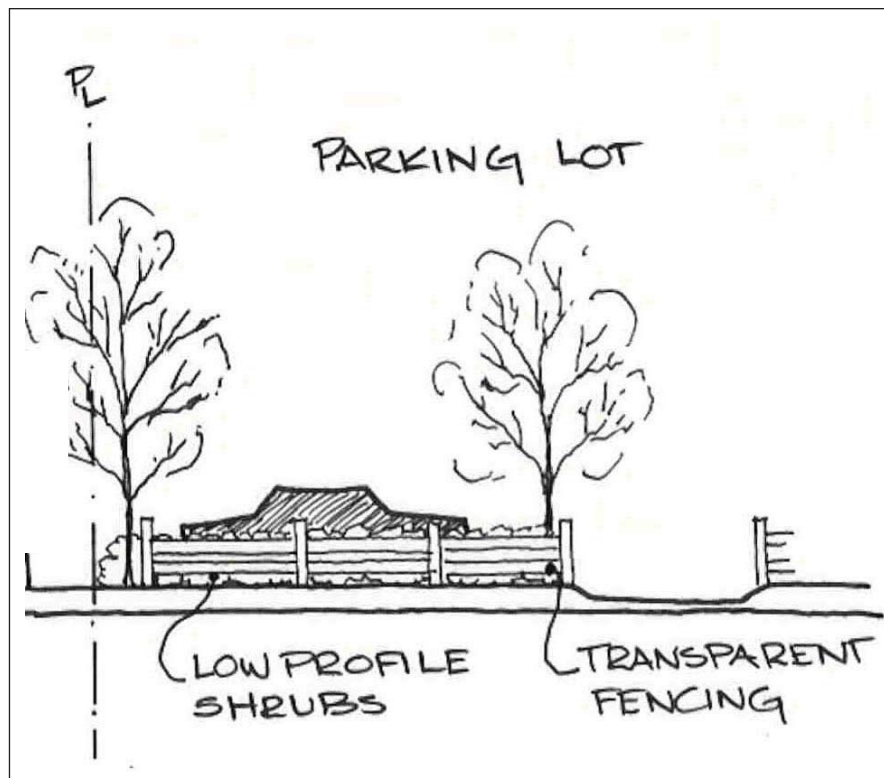
**Policy R19.** Lands identified on the Land Use Plan (Figure 1.3) as Medium Density Residential are considered appropriate for low profile, medium density, multi-dwelling development to a maximum of three storeys and not exceeding 10 metres to the eave. Maximum density shall be 125 units per hectare (50 units per acre). Total height to the peak of the roofline shall not exceed 13 metres.

### Redesignation

**Policy R20.** A land use redesignation and Area Redevelopment Plan amendment, will be required for medium density residential development proposed in locations not identified in the Land Use Plan (Figure 1.3).

### Parking

**Policy R21.** To encourage a pedestrian oriented environment, parking should be located to the rear, side yard or underground. Surface parking areas located exclusively in the front yard are discouraged. Where provided, front yard parking should be appropriately screened and treated in a pedestrian friendly manner.



**Policy R22.** Surface parking should be screened from adjacent residential development using a combination of low profile landscaping and/or fencing, while encouraging surveillance.

## **Landscaping**

**Policy R23.** Where on site landscaping is to be used for the purposes of screening parking areas, it should be sufficiently dense at the time of planting to provide for a reasonably continuous visual screen.

## **Trees**

**Policy R24.** The protection and preservation of existing trees is strongly encouraged. Relaxations to the requirements of the *Land Use Bylaw* may be appropriate in order to preserve existing mature trees.

**Policy R25.** Land owners and the City of Calgary are encouraged to work cooperatively to implement a continuous street tree canopy along the public right-of-way of Bowness Road and 46 Street. Street trees should be placed approximately 3 metres apart and located between the curb and the sidewalk in consistent alignment with the adjacent trees.

## **R-2A Land Use**

**Policy R26.** R-2A land use designations should be retained as these areas help provide a range of housing options. Redesignation to a lower density residential land use is not supported by this ARP.

## **Design**

**Policy R27.** Developments should be designed to be pedestrian friendly and demonstrate attention to detail and high quality design.

## **Design Guidelines**

**Policy R28.** Proposals for development within the areas identified as Medium Density Residential on the Land Use Plan (Figure 1.3) shall consider the Medium Density Residential Guidelines which follow. These design guidelines are not intended to restrict design flexibility or creativity but to ensure that design diversity is achieved in a manner consistent with the plan vision, objectives and policies.

## MEDIUM DENSITY RESIDENTIAL DESIGN GUIDELINES

Guideline R15. Medium density residential development should demonstrate sensitivity to community context by considering:

- (i) pedestrian orientated scale and massing, for example, by dividing apartment buildings into smaller-scale components and stepping back upper storeys along public streets;
- (ii) quality and attention to detail, particularly at the ground level, for example by adding bay windows, recessed or projecting balconies, verandas, and other elements that improve the visual interest, scale and character;
- (iii) design elements such as front porches or townhouses that encourage human interaction.



Guideline R16. Corner site development should reflect its dual frontage by presenting an attractive façade to both the frontage and flanking roadways.

Guideline R17. In order to enhance the street environment, the primary entry to the building and to as many individual townhouse units as possible should be visible from the front of the property.



*Primary entranceways should be located at the front.*

Guideline R18. Where possible, existing mature trees should be retained. Planting trees and vegetation beyond the *Land Use Bylaw* minimum requirements is encouraged.

Guideline R19. The minimum landscaping area requirements of the *Land Use Bylaw* should be principally met by soft rather than hard landscaping, including planted areas of vegetation including flower beds, shrubs, trees and hedges .

Guideline R20. The design of buildings and landscaping should adhere to the principles of Crime Prevention Through Environmental Design (CPTED). The key principles are :

(i) Natural Territorial Reinforcement

Delineating private from public space reinforces perceptions of ownership. People tend to protect the space they identify as their own and will be more likely to identify intruders and report them. Fences, pavement treatments, art, signs, lighting and landscaping help to clearly define private space.

(ii) Natural Surveillance

Criminals don't want to be seen. Landscaping and lighting should be planned to promote natural surveillance from inside the building and from outside by neighbours. Activity nodes can be planned and located to encourage natural surveillance. Visual obstacles and places of concealment should be minimized.

(iii) Access Control

Access points to a building should be easily observable. Place entrances, exits, fencing, landscaping and lighting in areas that encourage natural surveillance at access points.

Guideline R21. In situations where medium density residential and low density residential development share a rear alley, medium density residential development should address the potential for off site impacts by:

- (i) screening parking areas with high quality fences and prevent headlight glare;
- (ii) screening storage and garbage receptacles with fencing or dense vegetation;
- (iii) site lighting should be shielded and of moderate intensity.

## HIGH DENSITY RESIDENTIAL POLICY

**Policy R29. High density residential development is not considered appropriate. The Plan has sought to accommodate increased residential density through sensitive medium density intensification near the commercial core.**



*One of the main focuses of the ARP is to encourage the revitalization of the existing Bowness Road/46 Street commercial area to a pedestrian-friendly, mixed-use commercial/residential area similar to the example in this photograph (Garrison Woods).*

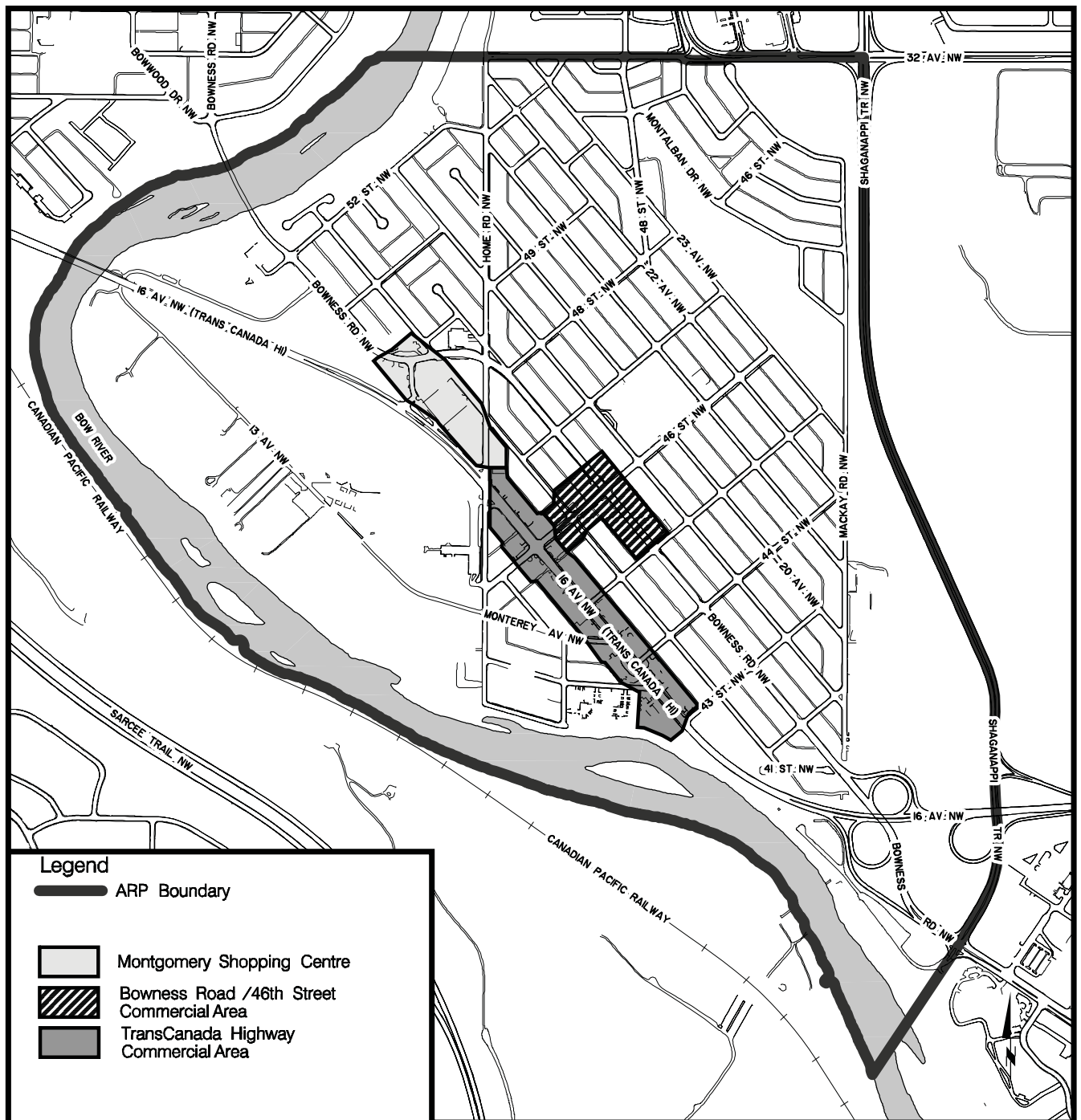
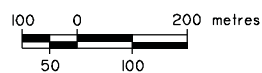


FIGURE 3.1

## MONTGOMERY COMMERCIAL AREAS



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## **BACKGROUND**

- 3.1 The community of Montgomery contains three distinct commercial areas (refer to Figure 3.1). These areas provide commercial services for both the local population and the travelling public. For information regarding the issues and concerns associated with these commercial areas, please refer to the Background Information Section.

### **Bowness Road & 46 Street Commercial Area**

- 3.2 Currently this area contains a mixture of local and regional commercial uses in an auto-oriented atmosphere with few amenities for pedestrian and transit users. This area is recognized as having strategic importance to the future of the community. In the long term, the area is envisioned as a pedestrian friendly mixed-use node providing both convenience commercial services and speciality services.

### **Trans Canada Highway Commercial**

- 3.3 The Trans Canada Highway (TCH) strip is currently designated as C-6 Highway Commercial. This area contains commercial uses that serve the motoring public. While the TCH is an important transportation route for Calgary, it bisects the community and isolates the southern residential area of Montgomery and Shouldice Park from the remainder of the community to the north.
- 3.4 Eventually the TCH will be widened to the south and in the very long term a ring road will be constructed around Calgary. Even when a by-pass is constructed, motorists will continue to use the Trans Canada Highway through Montgomery to access the downtown. The TCH will remain a gateway to Calgary into the future and reinforces the need for quality commercial development in this area.

### **Montgomery Shopping Centre**

- 3.5 This commercial area contains a large number of commercial operations. The largest business operation is Safeway, which underwent a significant expansion in 1998. At that time a number of long standing site circulation concerns were addressed. Other commercial operations include businesses in the Montgomery Shopping Centre strip mall, the Dairy Queen and in the strip mall paralleling Bowness Road at the western end of the community.



*Sketch of Existing Bowness Road/46 Street Commercial Area*



*Artistic Impression of Future Vision of Bowness Road/46 Street Commercial Area.*

**Note: This sketch is provided for illustration purposes only and does not represent an approved scheme.**

## COMMERCIAL VISION FOR THE FUTURE

3.6

*All of Montgomery's commercial areas have been enhanced and as a result have helped improve the image of the community. The Bowness Road/46 Street Commercial Area has evolved into a mixed-use commercial/residential area with an enhanced pedestrian environment. This area feels like a small town main street. Commercial uses located in the Montgomery Shopping Centre Area and along the Trans Canada Highway have contributed to an improved gateway to the community and Calgary.*

## COMMERCIAL LAND USE AND DEVELOPMENT OBJECTIVES

3.7

- (a) To ensure that Montgomery's image is enhanced and gateways celebrated.
- (b) To support the long-term development of the Bowness Road/46 Street Commercial Area as a pedestrian friendly mixed-use area that reinforces Montgomery's small town character.
- (c) To reduce land use conflicts and to concentrate commercial activities in existing commercial areas.
- (d) To encourage commercial development that is compatible in character and scale with adjacent residential areas.

## GENERAL COMMERCIAL DEVELOPMENT POLICIES

*The following policies relate to all commercial lands identified on Figure 3.1.*

### Commercial Land Use Expansion

Policy C1.

In order to secure the viability of existing commercial areas, the expansion of commercial uses into residential areas should not be permitted.

## **Parking**

- Policy C2.** Except where provided in this Plan, the parking requirements of *The Land Use Bylaw* are considered appropriate for commercial development.  
**Bylaw 38P2008**

## **Surface Parking Treatment**

- Policy C3.** Where surface parking areas meet the public sidewalk edge, screening with low level shrubs and/or durable decorative fencing should be required to improve area aesthetics.

## **Elevations**

- Policy C4.** All elevations facing streets and residential areas must be treated in a manner consistent with the front elevation. Blank or unarticulated facades are not acceptable.

## **Commercial Residential Interface**

- Policy C5.** In situations where commercial and residential development share a rear alley, commercial development should address the potential for off-site impacts on adjacent residential uses by:
- (i) screening the commercial parking area with high quality, fencing that will prevent headlight glare;
  - (ii) storage and garbage receptacles should be screened from nearby residential using fencing or dense vegetation;
  - (iii) site lighting should be shielded, of moderate intensity and not create adverse glare for nearby residential properties;
  - (iv) site surveillance of the rear lane should be considered in the design of the commercial buildings.

## **Signage**

**Policy C5.** Signage should be designed to improve the image of Montgomery and to reinforce the community's role as a major entryway to Calgary. Attention to detail is encouraged as well as the creation of pedestrian oriented signage. The following signs should not be permitted in any commercial area:

- (i) roof top signage;
- (ii) flashing or animated signs;
- (iii) rotating signs.

## **Free Standing Signs**

**Policy C6.** In order to improve aesthetics, shrubs, planters, planter beds for shrubs and flowers are encouraged at the base of free-standing signs. Relaxations to the maximum height as established in the *Land Use Bylaw* shall not be granted.

## **Commercial Laundry Facilities**

**Policy C7.** Commercial laundry facilities are not considered appropriate use due to their industrial nature in terms of noise, air emissions and visual impact.

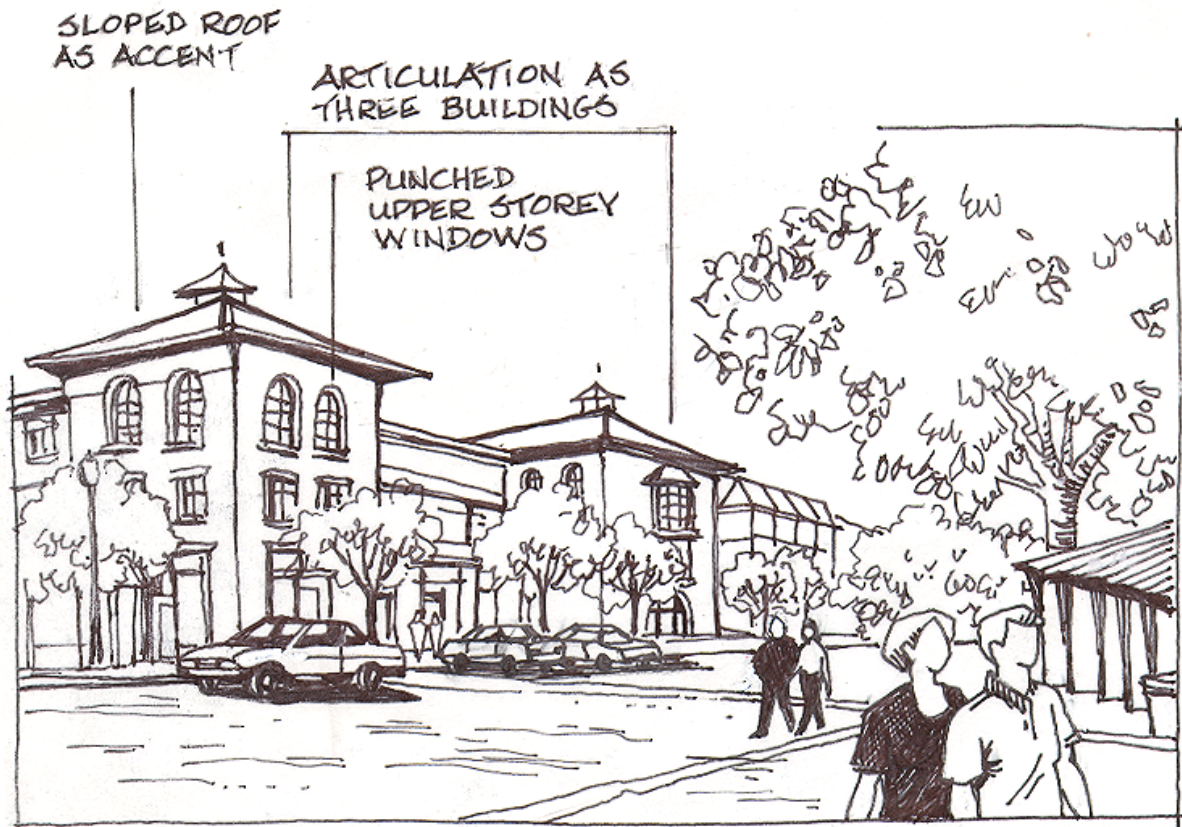
## **Commercial and Mixed Use Design Guidelines**

**Policy C8.** Proposals for development within the commercial areas identified on Figure 3.1 should demonstrate consistency with the Commercial and Mixed Use Design Guidelines which follow. These design guidelines are not intended to restrict design flexibility or creativity but to ensure that design diversity is achieved in a manner consistent with the plan visions, objectives and policies.

## COMMERCIAL AND MIXED-USE DESIGN GUIDELINES

Guideline C1. Building masses should be pedestrian oriented and respond to a 'human scale' with details that are proportional to human height and provide visual interest at the street and sidewalk levels. Buildings should be reduced in apparent mass or articulated to avoid large monolithic box-like structures. This can be achieved by:

- (i) use of different building materials, colour and/or texture;
- (ii) variations in roof forms, height of roof elements or cornice detailing;
- (iii) designs that emphasize floor lines, through terracing, articulated structural elements or a change in materials and horizontal trim bands to define floors;
- (iv) designs that express rhythms and patterns of windows, columns and other architectural features.



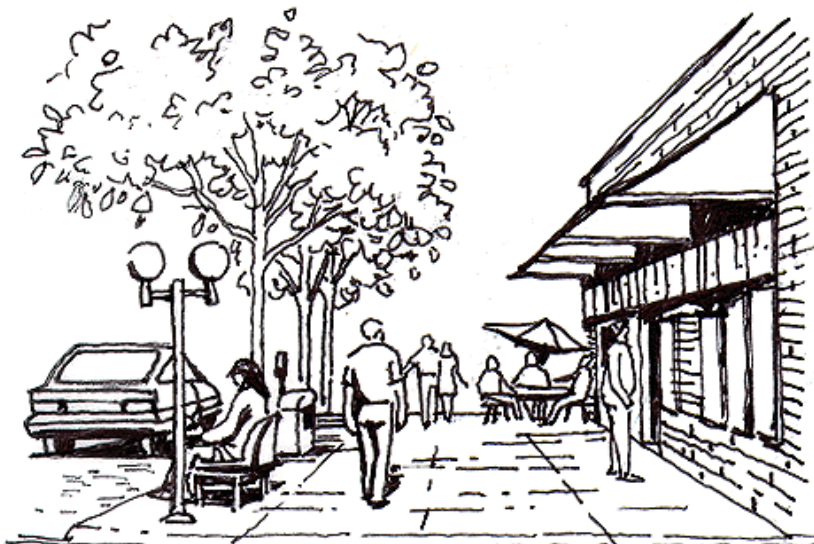
Guideline C2. Blank walls, particularly at the ground level should be avoided.



*Blank walls, particularly at ground level are discouraged.*

Guideline C3. Windows that reveal indoor amenities, activities and displays are encouraged. Large expanses of opaque or reflective windows and ceiling to floor windows are discouraged, especially in the Bowness Road and Montgomery Shopping Centre Commercial Areas.

Guideline C4. Canopies or awnings are encouraged especially in the Bowness/46 Street commercial area and the Montgomery Shopping Centre.



*Canopies & awnings provide weather protection for pedestrians.*

Guideline C5. Where commercial or mixed-use development is located adjacent to a residential area, building design and form should be configured to minimize adverse impacts, including massing, shadowing, over looking and loss of privacy.



*In this example, the impact of massing is minimized through a step down building design.*

Guideline C6. Entrances should be a primary element of the street frontage. Main entrances should be clearly identifiable and inviting.

Guideline C7. As part of the '88 Winter Olympic improvements, the following design suggestions were recommended in Montgomery – A Handbook of Improvements and Suggestions Trans Canada Highway – West (1986). The following guidelines apply only to development along the TCH and should be considered:

- (i) consistent boulevard deciduous tree planting on both sides of the TCH through Montgomery;
- (ii) low level vegetation planted in corner visibility triangles in order to enhance site aesthetics;
- (iii) use of site landscaping to define the site entrance and circulation for both vehicles and pedestrians;
- (iv) define the corridor and enhance site aesthetics through treatment of the front property line. This can be achieved through the installation of a low-level decorative fence at the front property line. The fence should be constructed of durable material and should be designed to complement the building face treatment. Use of translucent materials and transparency in design is encouraged in order to reinforce opportunities for natural surveillance. Individuality in materials and construction is also encouraged.

## BOWNESS ROAD/46 STREET COMMERCIAL AREA POLICIES

*In addition to the General Commercial policies, the following policies relate to the lands identified on Figure 3.1 as the Bowness Road/46 Street Commercial Area.*

### Residential/Commercial Mixed Use

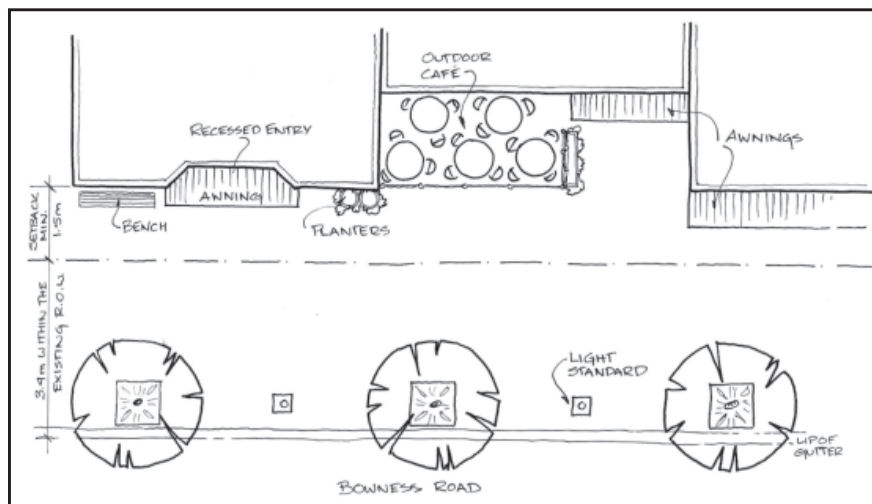
- Policy C9.** This Plan supports and encourages residential uses located above commercial uses in the Bowness Road/46 Street Commercial area.

### Minimum Commercial Component

- Policy C10.** For commercial/residential mixed-use developments, the first floor should provide for active uses such as small scale retail outlets, personal service businesses and restaurants which provide store front access off the sidewalk from Bowness Road or 46 Street.

### Height

- Policy C11.** In order to support the viability of the commercial core, development to the maximum height is encouraged. The maximum height shall be 12 metres to the eave and 15 metres to the roofline, and no more than three storeys. However, a bonus fourth storey, to a maximum height of 15 metres to the roofline (with no restriction to eave height) may be allowed on the 4600 block on the western side of Bowness Road NW if substantial contributions to the pedestrian environment are achieved. These shall include (but not necessarily be limited to) the provision of building step-backs (using a minimum area of 20% of the ground floor building footprint), street furniture, landscaping, signage, and detailed building design oriented to the pedestrian.



*Refer to Policy C12.*

*Note: This sketch is provided for illustration purposes only and does not represent an approved scheme.*

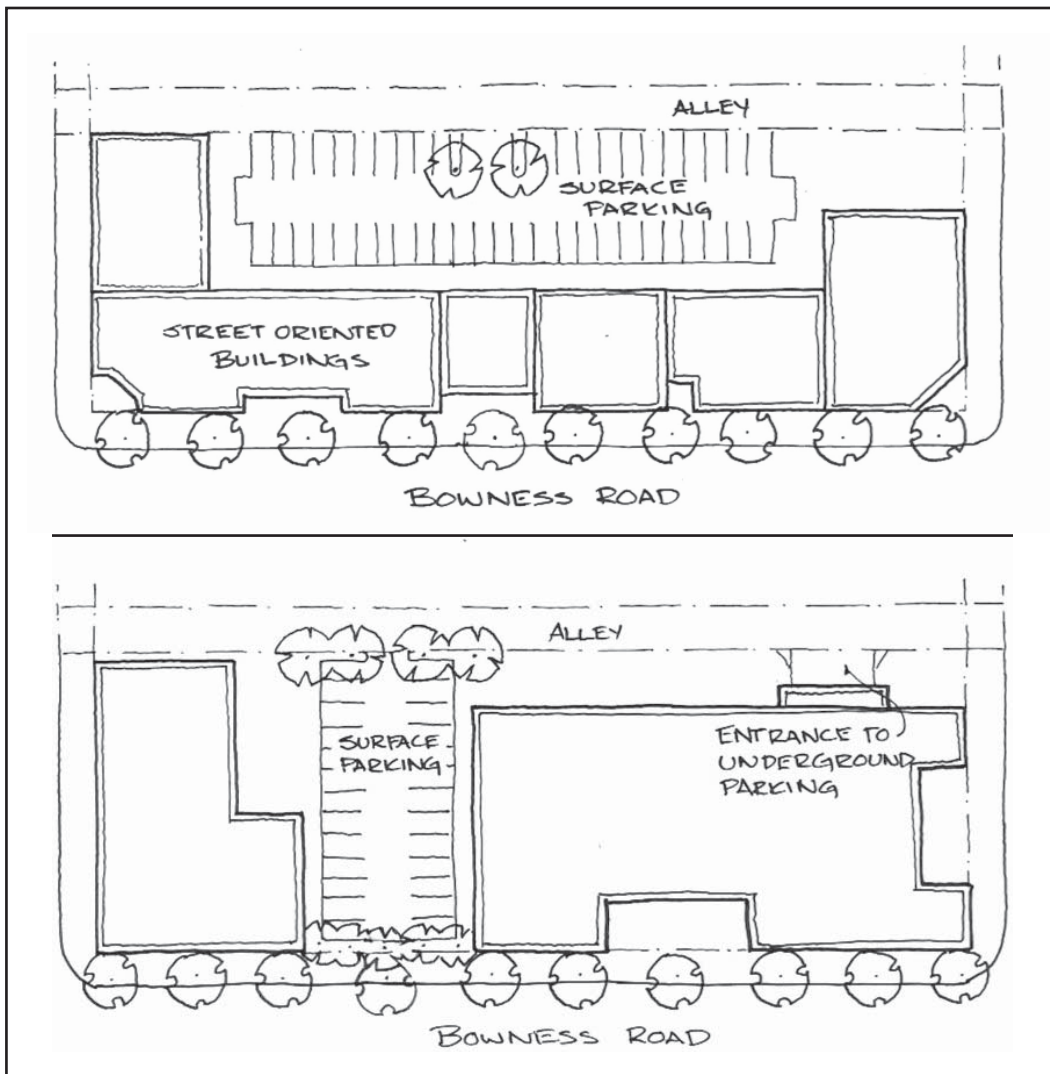
## Automobile Oriented Uses

**Policy C12.** Automobile oriented uses, including automotive sales, automotive rentals, automobile workshops, car washes, gas stations, and drive through services/restaurants should not be permitted in the Bowness Road/46 Street Commercial Area. These uses are not conducive to the creation of a pedestrian friendly shopping and service area.

## Bowness Road Setback Requirements

**Policy C13.** In order to assist in creating a pedestrian oriented environment, development abutting Bowness Road should be set back 1.5 metres from the basic right-of-way (as defined in *The Land Use Bylaw Part 3, Division 1*). The front yard area should facilitate pedestrian movement and should provide a seamless transition from the public sidewalk to private property.

**Bylaw 38P2008**



Refer to Policy C13.

*Note: This sketch is provided for illustration purposes only and does not represent an approved scheme.*

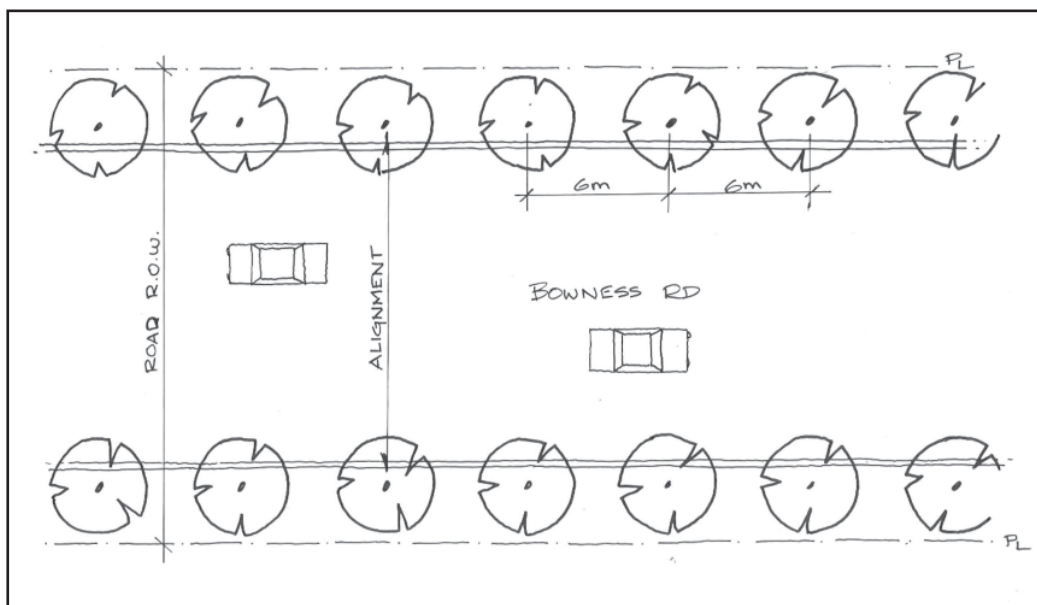
- Policy C14.** Where abutting Bowness Road, development setbacks greater than 1.5 metres from the basic right-of-way (as defined in *The Land Use Bylaw Part 3, Division 1*) are considered appropriate provided the larger front yard space is used to create recessed storefront entrances, special corner features and or usable open space (e.g. outdoor café or plaza). **Bylaw 38P2008**

## Parking

- Policy C15.** To encourage the development of the Bowness Road/46 Street Commercial Area as a pedestrian-oriented area, parking areas should locate to the rear, the side yard, or underground. Surface parking areas located exclusively in the front yard are discouraged. Where provided, front yard parking should be appropriately screened and treated in a pedestrian friendly manner.

## 46 Street Setback Requirements

- Policy C16.** In order to assist in creating a pedestrian oriented environment development abutting 46 Street should be built to the property line immediately adjacent to the 46 Street sidewalk. Setbacks are considered appropriate only where the larger front/side yard space is used to create recessed storefront entrances, special corner features and/or usable open space (e.g. outdoor cafe or plaza).



Refer to Policy C16.

**Note:** This sketch is provided for illustration purposes only and does not represent an approved scheme.

## **Design**

- Policy C17.** New commercial development shall contribute to the creation of a high quality pedestrian environment by addressing building design, signage, façade treatment, landscaping and street amenities (furniture, street lighting, flower baskets).

## **Street Trees**

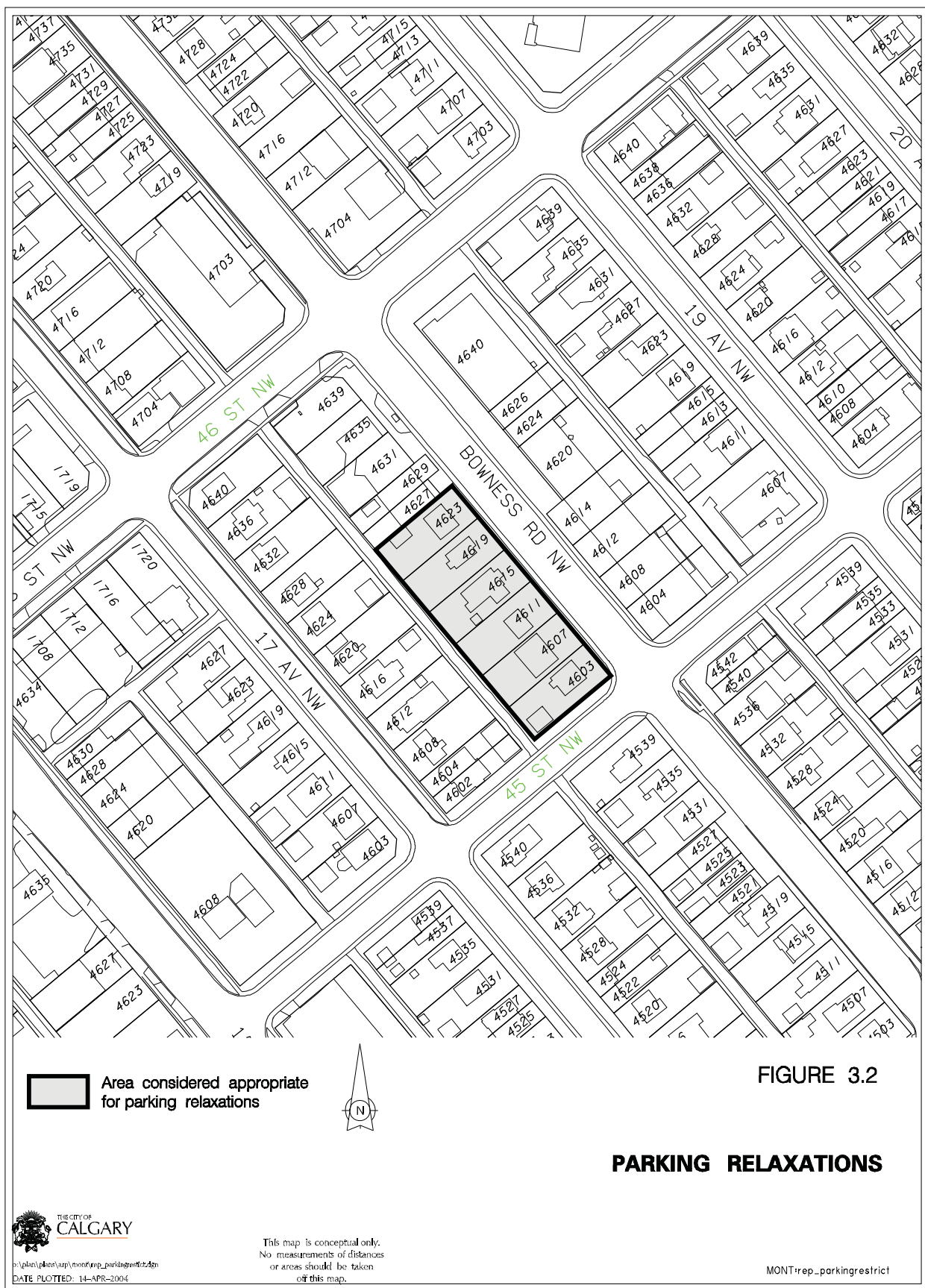
- Policy C18.** Property owners and the City of Calgary are encouraged to work cooperatively to implement a continuous street tree canopy along the public right of way of Bowness Road and 46 Street. Street trees should be located between the curb and the sidewalk, placed 6 metres apart and in consistent alignment with adjacent street trees.

## **Special Parking Consideration**

- Policy C19.** A 25% parking relaxation may be considered appropriate for the adaptive reuse of single detached dwellings identified on Figure 3.2. This relaxation is considered appropriate only for uses such as small scale retail businesses or restaurants (for which the majority of customers come from Montgomery or nearby communities) that make a contribution to the social gathering function of this area. Relaxations should not be provided for uses such as medical or veterinary clinics or offices. At the time of comprehensive redevelopment of these lands, parking relaxations will not be considered appropriate.

## **Existing Commercial Laundry Facility**

- Policy C20.** The commercial laundry facility at 4708, 4212, 4716 and 4720 17 Avenue NW has closed down. This use is no longer considered acceptable (refer to Policy C7). Medium density residential development or mixed use commercial residential development are considered to be appropriate uses for this site (subject to a satisfactory environmental assessment).



## **TRANS CANADA HIGHWAY (TCH) COMMERCIAL AREA POLICIES**

*In addition to the General Commercial policies, the following policies relate to the lands identified on Figure 3.1 as the Trans Canada Highway Commercial Area.*

### **Design**

- Policy C21.** Development should reflect its function as a gateway to the community of Montgomery and to Calgary through high quality design.

### **Site Landscaping**

- Policy C22.** In order to enhance the gateway function of the Trans Canada Highway, site landscaping treatment should support the “City by the Rockies” theme reflected in *Improving Calgary’s Entranceways* (1994). (A summary of the City by Rockies theme is provided in the Background Section for ease of reference).

### **Landscaping Requirements**

- Policy C23.** In order to enhance the appearance of the Trans Canada Highway developments are encouraged to exceed the minimum landscaping requirements of *The Land Use Bylaw*. Relaxations should not be granted. The use of soft landscaping is preferred, including grass, shrubs, trees and decorative flower borders.  
**Bylaw 38P2008**

### **RV/Automobile Dealerships**

- Policy C24.** In accordance with *Improving Calgary’s Entranceways* (1994), recreational vehicle or automobile dealerships should:
- (i) setback inventories (for example, cars) 2.1 metres from the property line and screen inventories with a one metre high fence constructed of high quality materials;
  - (ii) provide landscaping adjacent to the roadway in order to provide visual interest and to soften the appearance of the fence and inventories;
  - (iii) allow only one non-screened vehicle display area suitable for up to two recreational vehicles or automobiles.

## **MONTGOMERY SHOPPING CENTRE POLICIES**

*In addition to the General Commercial policies , the following specific policies apply to the area identified on Figure 3.1 as the Montgomery Shopping Centre.*

### **Design**

- Policy C25.** Development should reflect its function as a gateway to the community of Montgomery and to Calgary through high quality design.

### **Site Landscaping**

- Policy C26.** In order to enhance the gateway function of the Trans Canada Highway, site landscaping treatment should support the "City the Rockies" theme reflected in *Improving Calgary's Entranceways* (1994). (A summary of the City by the Rockies theme is provided in the Background Section for ease of reference).

### **Elevations**

- Policy C27.** All elevations facing streets and residential areas must be treated in a manner consistent with the front elevation. Blank or unarticulated facades are not acceptable.

### **Sites Abutting Bowness Road**

- Policy C28.** For redevelopment of sites abutting Bowness Road Policies C15 and C18 relating to parking and street tree placement also apply.

### **Recycling Centres**

- Policy C29.** Shopping Centres can provide a convenient location for small scale recycling facilities including bottle, paper, can, textile and plastic barriers storage banks. The Plan supports the provision of such facilities subject to adequate environment safeguards.

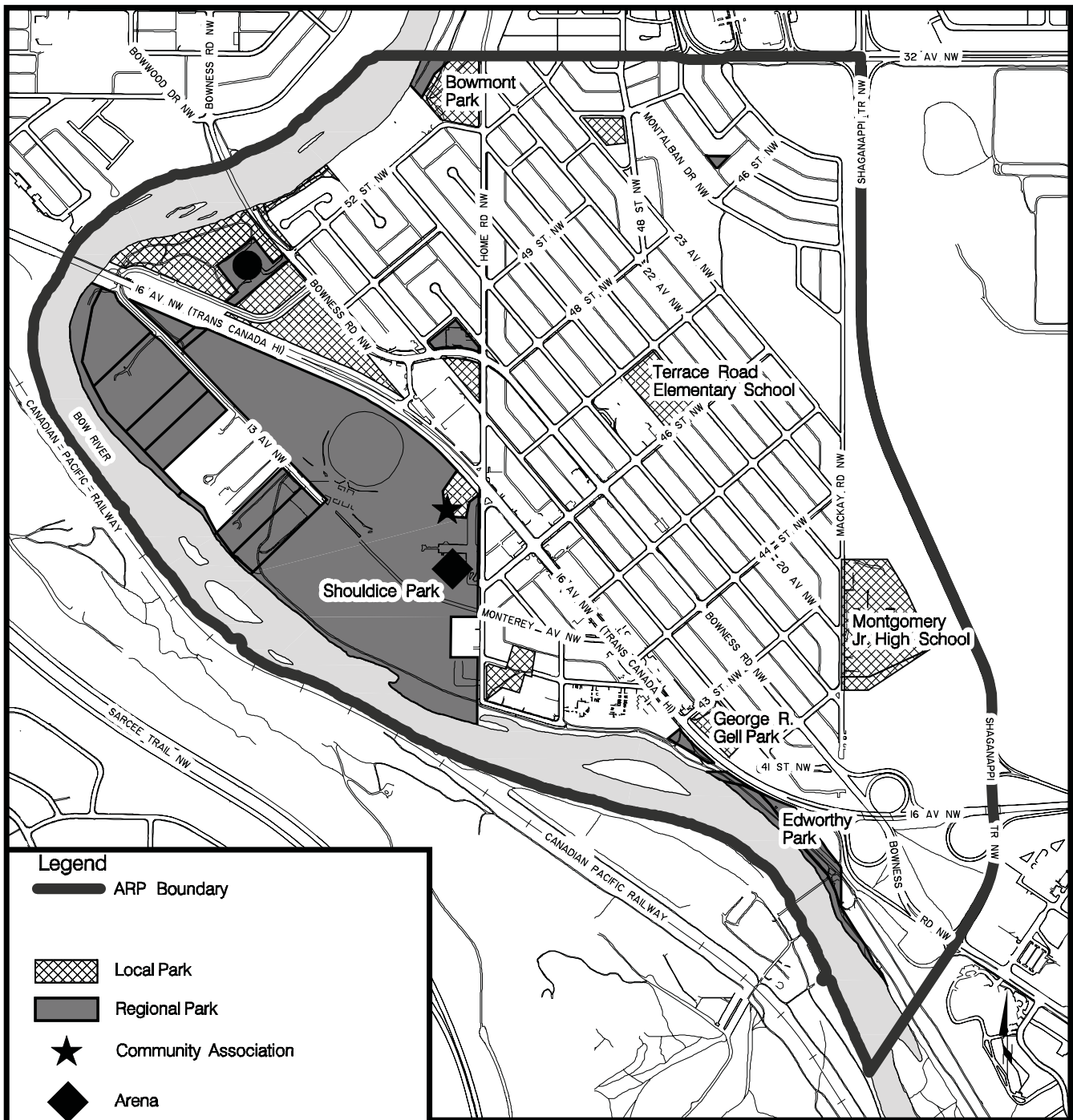


## Chapter 4

## Parks, Schools & Community Facilities



*Montgomery is well served by a system of local and regional park space, two-schools, and a range of recreational opportunities. The above photographs show unnamed local park space (1 & 2), Shouldice Regional Park (3) and Montgomery Junior High School (4).*



## **CONTEXT**

- 4.1 Parks, Schools and Community Facilities are important amenities that help create attractive and liveable neighbourhoods. Montgomery is well served by a system of local and regional parks, two school sites and a range of recreational opportunities (including Shouldice Swimming Pool, tennis courts and arena). Residents consider Montgomery as a special place because of its existing parks. It is an important objective of this Plan to preserve and enhance Montgomery's parks.

## **REGIONAL PARK SPACE**

- 4.2 Montgomery presently contains approximately 37.79 hectares of Regional Park Space which includes Shouldice Park, Bowmont Park, Edworthy Park and Part of the Bow River System.

## **LOCAL PARK SPACE**

- 4.3 Currently Local Park Space covers only 5.8% of the net community district area<sup>1</sup>. This is well below the city's minimum standard of 10%. The distribution of local park space is also recognized as a major concern. Terrace Road Elementary School provides an important centrally located open space and efforts should be taken to preserve this area for local park space if disposition were to occur.

## **SCHOOLS**

- 4.4 Presently, Montgomery contains two public schools, Terrace Road Elementary and Montgomery Junior High. The Calgary School Board has approved policies and procedures to address issues, in conjunction with parents, school staff, and the Community Association, should redevelopment of these sites occur.

## **OBJECTIVES**

- 4.5
- (a) To retain and enhance existing parks and encourage their use for a variety of recreational, leisure and cultural activities.
  - (b) To improve the distribution of local park space.
  - (c) To encourage improvements to the Bow River pathway connections through the community of Montgomery.

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<sup>1</sup> The remaining area of a community district once regional park space has been deducted.

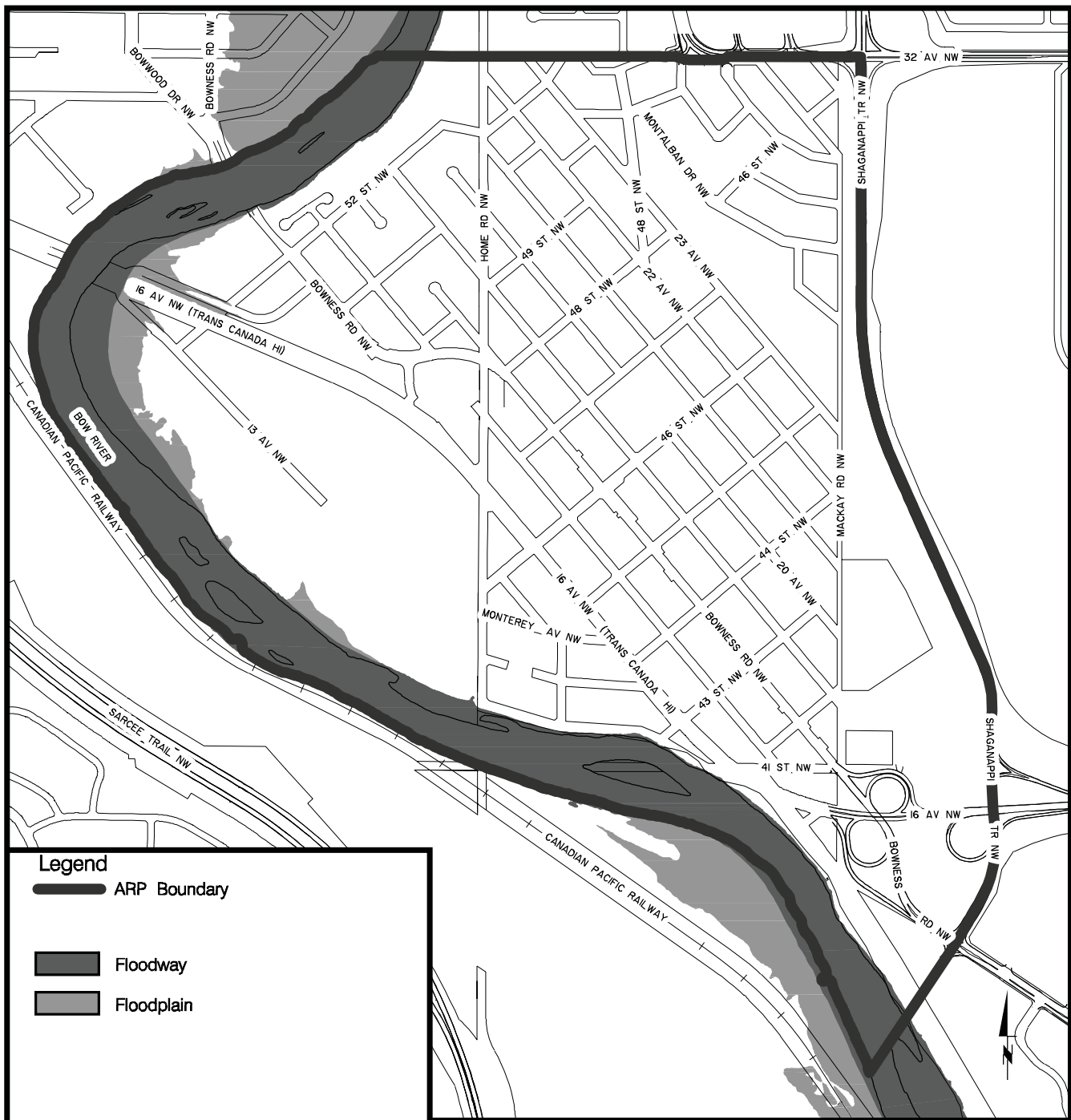


FIGURE 4.2

## FLOODPLAIN\FLOODWAY

- (d) To protect and preserve ecologically and environmentally sensitive areas for the benefit, use and enjoyment of current and future residents.
- (e) To establish visual gateways into Montgomery through landscaping and signage improvements.
- (f) To consider the recreational needs of aging residents in future parks improvements.

## **PARK POLICIES**

### **Improvements**

- Policy P1.** Parks, recreational and community facilities should be improved in cooperation with residents, the Community Association and The City of Calgary. Future improvements to Montgomery's parks should be undertaken with reference to the Action Section of this Plan.

### **Aging Population**

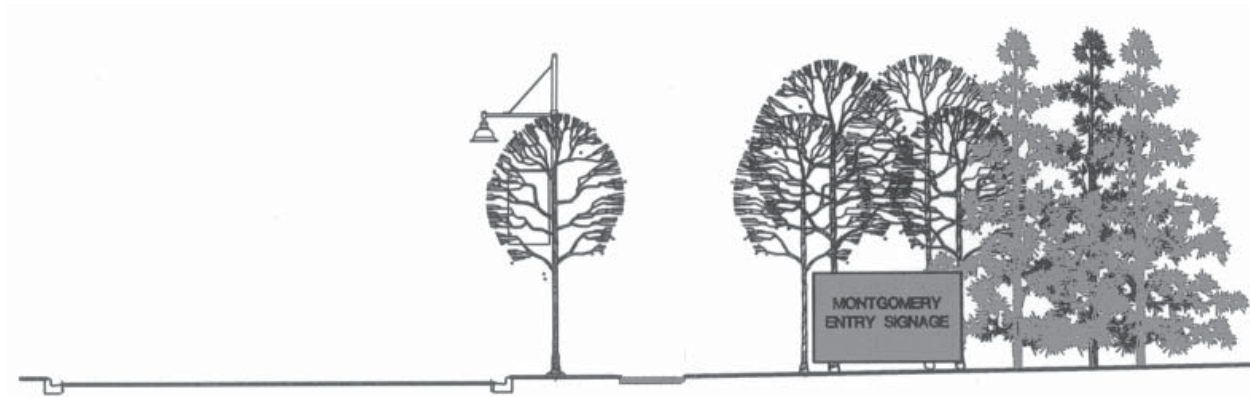
- Policy P2.** The needs of the aging population should be integrated into parks improvement strategies.

### **Pathways**

- Policy P3.** The City of Calgary should secure, on an opportunity basis, pathway linkages to complete the Bow River Pathway system as it runs through Shouldice Park and Montgomery.

## Gateways

- Policy P4.** Tree planting is encouraged to enhance gateways into Montgomery. Gateway priorities include lands adjacent to the Trans Canada Highway and Bowness Road. The Community Association is encouraged to install community identification signs at all entry points.



## George Gell Park

- Policy P5.** The proposed pedestrian overpass of 16 Avenue at 43 Street will impact George Gell Park. The City of Calgary will work cooperatively with the Community Association and area residents to address concerns created by the development of the pedestrian overpass.

## Shouldice Park - 13 Avenue

- Policy P6.** In accordance with the *Urban Parks Master Plan* (1994), The City should acquire, on an opportunity basis, the remaining privately held parcels adjacent to 13 Avenue for the purposes of Shouldice Park expansion.

## Tree Planting

- Policy P7.** Tree planting is encouraged adjacent to the Trans Canada Highway in order to enhance the entryway to Montgomery as well as to Calgary.

## **Ecological and Environmentally Sensitive Areas**

- Policy P8.** The floodplain of the Bow River (refer to Figure 4.2) should be preserved and protected in its natural state to ensure the ecological and hydrological components are maintained in perpetuity. Access for passive recreational uses should be limited to strategic locations to ensure the valuable riparian habitat and instream fisheries habitat are protected from conflicting recreational uses (also refer to Figure 4.3).

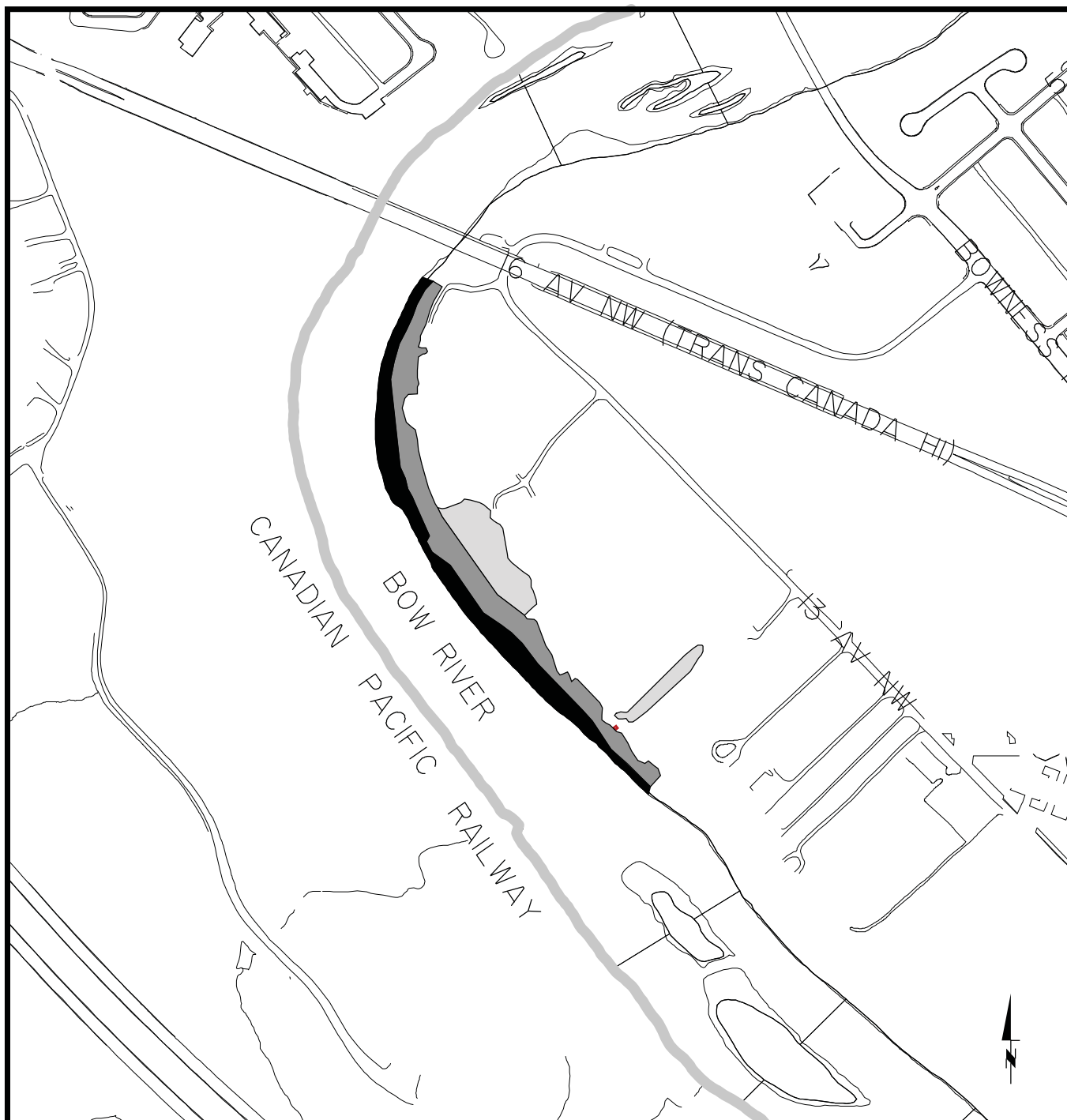
## **School Sites**

- Policy P9.** If the Terrace Road Elementary School is declared surplus by the Calgary Board of Education, The City of Calgary should consider exercising its right of first refusal in order to secure the School's playground and sport field area for local open space.

## ***Service Organization***

- Policy P10.** *In order to preserve the residential character of Montgomery, 1404 Home Road NW should only be a service organization that is compatible with the surrounding area in terms of design, scale and intensity.*

**Bylaw 19P2008**



# Legend

- Non-Native Grassland
- Balsam Poplar
- Riverine Tall Shrub

FIGURE 4.3

## ENVIRONMENTALLY SIGNIFICANT AREAS



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*Bowness Road has been identified as a priority for enhancement (1), the Trans Canada Highway runs through the community (2), the Plan seeks to improve bike and pathway links and supports pedestrian overpasses over the Trans Canada Highway and Shaganappi Trail NW (3 & 4).*

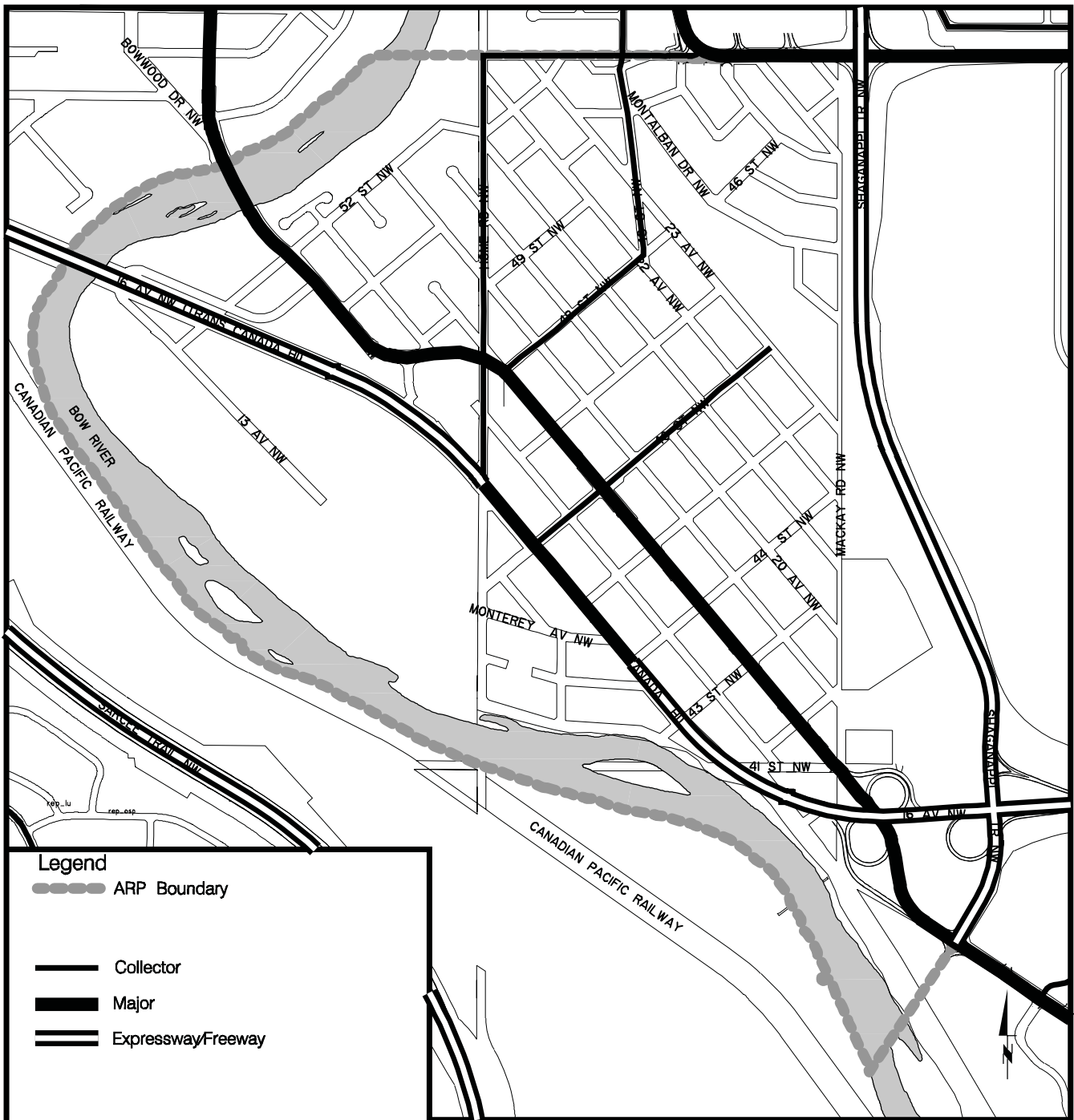
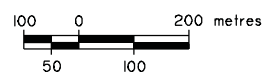


FIGURE 5.1

## ROAD NETWORK



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## **ROAD**

### **Network**

- 5.1 Montgomery is well-served by a hierarchy of roads. Both a freeway/expressway (the Trans Canada Highway) and a major road (Bowness Road) run diagonally through the community. Another freeway/expressway (the Shaganappi Trail) runs along the eastern border linking into both Bowness Road and the Trans Canada Highway. Home Road NW is another major road which runs north south through the middle of the community. There are several major connections to the Trans Canada Highway including a major interchange at the far southern end of the plan area which also links into Bowness Road and the Shaganappi Trail. The designated collector streets in the community are linked to the major streets and have transit service. (Refer to Figure 5.1 and 5.2).

### **Community Traffic Study**

- 5.2 The City of Calgary and the Montgomery Community Association Traffic Committee completed a Community Traffic Study in conjunction with the preparation of this Plan. (See the Background Section for detailed information). The Traffic Study included extensive public consultation and a number of transportation concerns were identified.
- 5.3 The most frequently mentioned traffic problems were related to excessive speed, high traffic volumes, high truck volumes, cyclist vehicle conflicts, parking issues and pedestrian safety. The most frequently mentioned locations to which these problems related included 16 Avenue (Trans Canada Highway), Bowness Road, Home Road, 32 Avenue, 48 and 52 Streets. The results also indicated that problems occurred at these locations everyday and were not restricted to rush hour times.
- 5.4 Most of the problems raised are caused by traffic shortcutting through the community. In particular, the alignment of the Trans Canada Highway through Montgomery and the connections between this roadway and the Shaganappi Trail has resulted in a shorter distant route via Home Road and 32 Avenue NW. This situation is compounded by the lack of connections between Shaganappi Trail and the Trans Canada Highway for traffic that is orientated to or from the north and the west. Unless preventive measures are put in place shortcutting traffic volumes are likely to increase in the future due to several major developments in Northwest Calgary, including Market Mall expansion, Foothills Hospital expansion, the Childrens Hospital and the expansion of the University of Calgary Main Campus.

*Reducing the setback requirement on Bowness Road would enable redevelopment nearer to the sidewalk providing a more pedestrian friendly environment. Sidewalk improvements could be accommodated within the existing right-of-way.*

- 5.5 One of the main goals of the Traffic Committee was to identify solutions for reducing the traffic shortcutting through the community. Several traffic control options were identified and presented to the community through a Questionnaire Survey and Public Open House. Although the majority (over 50%) of respondents supported most of the traffic calming options put forward, only one option received sufficient support (over 60%) to meet The City's criteria to enable implementation (traffic calming on Home Road). Traffic shortcutting is therefore likely to continue to be a major problem for the foreseeable future.

## **Bowness Road**

- 5.6 Bowness Road has been identified as a priority for improvement from both a transportation and land use perspective. The enhancement of the commercial area on Bowness Road has been identified through the community planning process as having strategic importance to the broader community. It is envisioned that Bowness Road and specifically the commercial area should evolve to a mixed-use pedestrian friendly environment. Traffic calming efforts on Bowness Road would serve to support transportation goals as well encourage commercial area revitalization.
- 5.7 Existing property setback requirements established in *The Land Use Bylaw* require a 5.2 metre (17 ft.) setback on both the north and south sides of Bowness Road. These setbacks are considered as a disincentive to reinvestment and do not support the long-term vision for the enhancement of the commercial area. An urban design concept based upon the general theme of 'Clean and Green' was prepared in conjunction with this ARP to provide a framework for the redevelopment of Bowness Road. The design work demonstrated that an enhanced pedestrian environment could be accommodated within the Basic Right of Way for Bowness Road.

**Bylaw 38P2008**

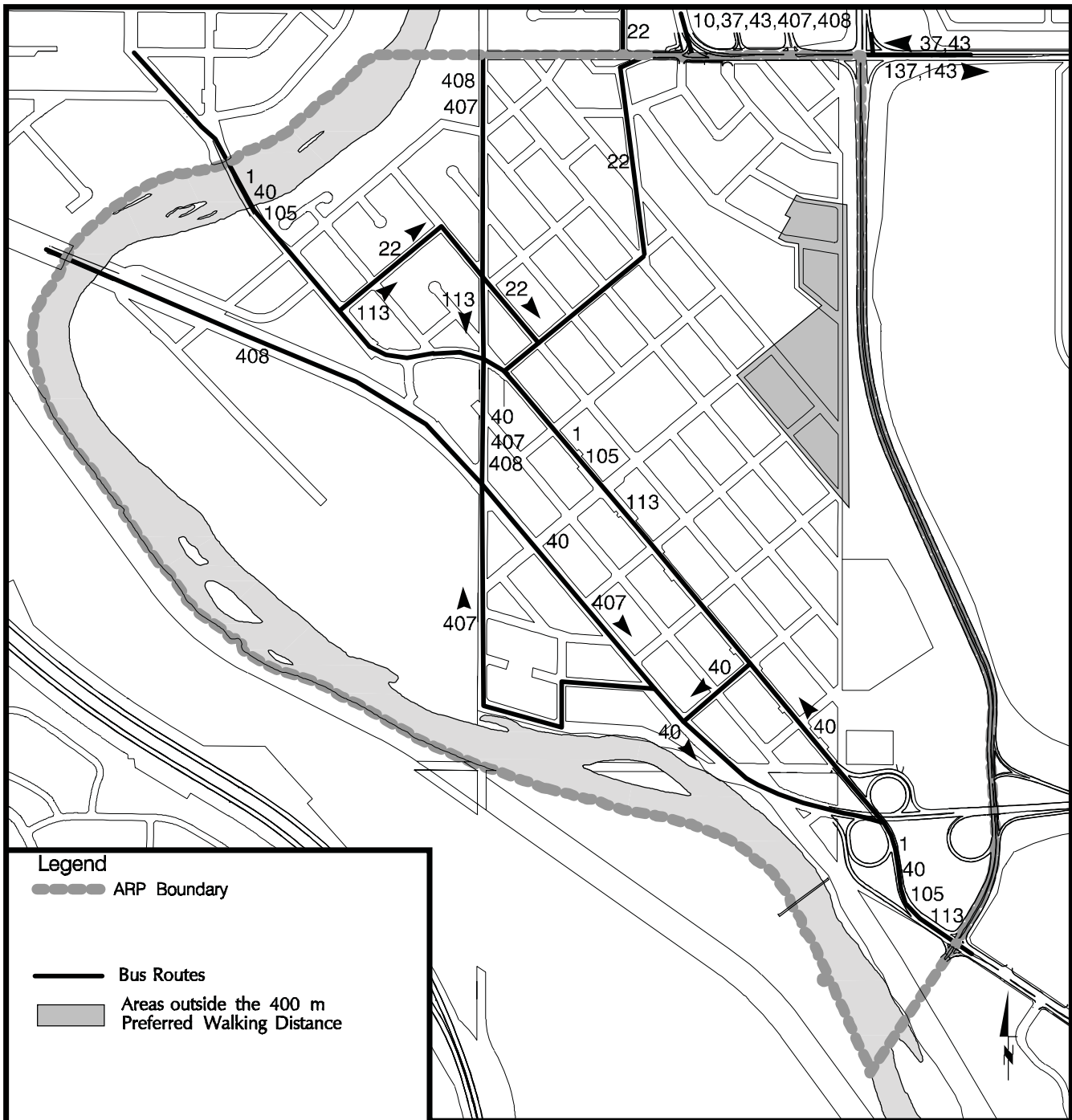


FIGURE 5.2

### EXISTING TRANSIT ROUTES



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## Bicycle and Pedestrian Pathways

5.8 Montgomery has a mixture of off street multi-use pathways and on street bikeways that are well connected to the main Bow River multi-use pathway which leads either to Downtown or to communities in the North West. The Hextall Bridge provides a multi use pathway across the Bow River to the community of Bowness. On street bikeways on Home Road and Mackay Road run north south connecting to 32 Avenue. 52 Street provides an on-street connection in the River Valley trail network (refer to Figure 5.3). There are a number of important considerations:

- (a) The creation of a river pathway link on the east side of Bow River behind the dwellings on 52 Street is currently constrained due to topographic conditions, and lack of resident support. 52 Street NW is currently designated as a one way on-street bicycle route. The development of a two-way cycle/pathway route is supported by this Plan.
- (b) Home Road is one of the busiest cycle routes in Calgary as it provides a direct connection from the north valley escarpment to the Bow River Pathway system.
- (c) A pedestrian overpass over 16 Avenue at 43 Street has been identified as one of the City's top priority overpass locations.
- (d) A pedestrian overpass of Shaganappi Trail linking Montgomery to the University Endowment Lands is proposed in the *Calgary Pathway & Bikeway Plan – North* and is supported in this Plan.



*Sketch of Existing Bowness Road.*



*Artistic impression of future vision of Bowness Road with streetscape improvements based on a “Clean & Green” theme.*

***Note: This sketch is for illustrative purposes only and does not represent an approved streetscape scheme.***

## Transportation Objectives

- (a) To address traffic safety, speed and quality of streetscapes through traffic calming measures.
- (b) To integrate cycling and pedestrian pathways into the existing transportation network.
- (c) To enhance the streetscape environment of Bowness Road.
- (d) To achieve a more 'workable' neighbourhood.

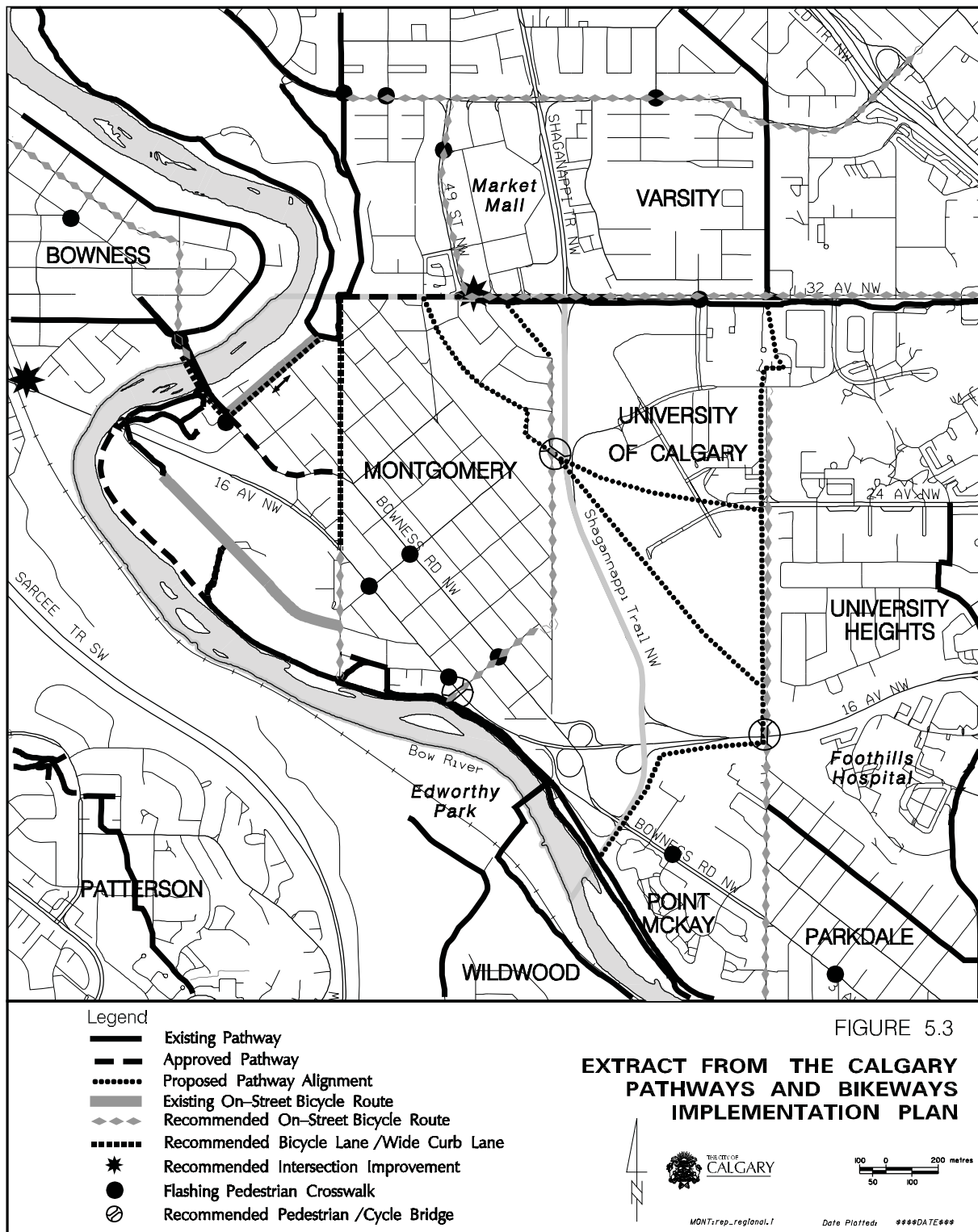
## TRANSPORTATION POLICIES

### Transit

- Policy T1.** The Plan supports development which encourages transit ridership as detailed in the *Transit Friendly Design Guide* (Approved 1995) including the provision of attractive and comfortable bus stops.

### Bowness Road

- Policy T2.** Transportation improvements to Bowness Road for the purpose of increasing volume capacity are not supported by this Plan.
- Policy T3.** The Plan recommends the reduction of the right-of-way property line setbacks set out in *The Land Use Bylaw Part 2, Division 1* along Bowness Road NW from 51 Street NW to McKay Road NW from 5.182 metres to 1.5 metres on each side.  
**Bylaw 38P2008**
- Policy T4.** The existing width of Bowness Road NW from 51 Street NW to McKay Road should be maintained. All remaining land within the basic right-of-way should be preserved for pedestrian and streetscape improvements on both sides of the street except where required for turning lanes or bus bays.
- Policy T5.** This Plan supports streetscape improvements to Bowness Road for the purposes of area revitalization and traffic calming. (See Action and Background Sections of this Plan).



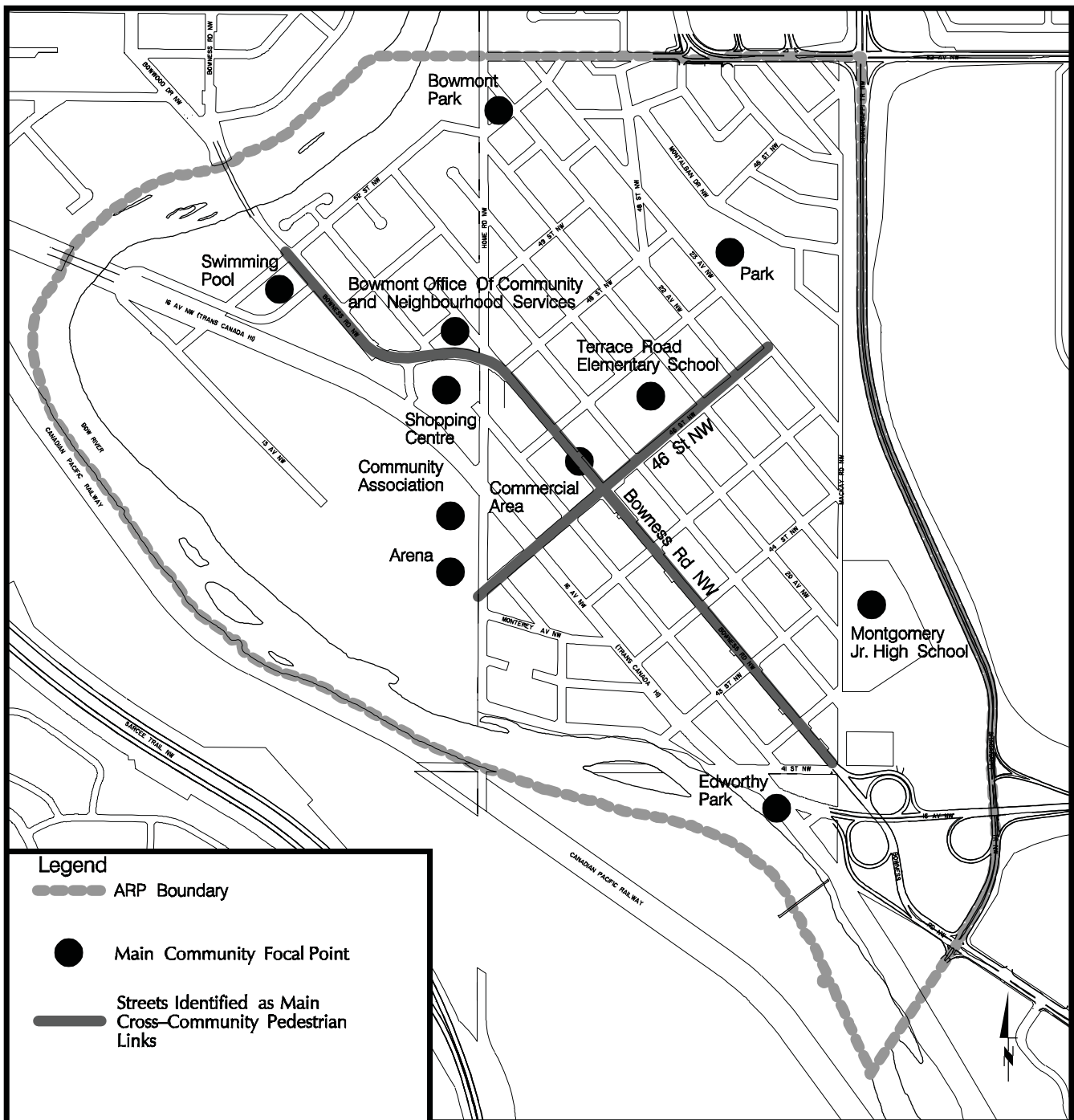


Figure 5.4

## MAIN COMMUNITY FOCAL POINTS AND CROSS-COMMUNITY PEDESTRIAN LINKS



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## **Sidewalk Provision**

- Policy T6.** Currently there is no sidewalk provided in front of the properties on the western side of 4500 Block of Bowness Road NW. The ARP supports the provision of a sidewalk at this location.

## **Calgary Pathways & Bikeways Implementation Plan**

- Policy T7.** The ARP supports the pedestrian and bikeway proposals identified in *The Calgary Pathways & Bikeways Implementation Plan* (refer to Figure 5.3).

## **Walkable Neighbourhood**

- Policy T8.** The ARP supports the concept of a 'walkable neighbourhood' where pedestrians are able to walk to and from focal points such as shopping areas, parks, schools and other community facilities (refer to Figure 5.4) in a safe, comfortable and attractive environment.

## **52 Street NW Two Way Bike/Pedestrian Route**

- Policy T9.** A two-way bike and pedestrian route is supported on 52 Street NW to establish a linkage from the Bow River pathway system to the north valley escarpment.

## **Pedestrian Overpasses**

- Policy T10.** At such time that a pedestrian overpass is considered for implementation over 16 Avenue, The City of Calgary will work with area residents and the Community Association in order to ensure that community impacts are considered during the design process.
- Policy T11.** At such time that a pedestrian overpass is considered for implementation over 16 Avenue The City of Calgary should consider opportunities to create an attractive community gateway through the provision of soft landscaping , including trees, shrubs and flowers.
- Policy T12.** At such time that a pedestrian overpass is considered for implementation over Shaganappi Trail, The City of Calgary will work with area residents and the Community Association in order to address relevant impacts including the potential on-street parking concerns in the vicinity of the pedestrian overpass.

## **Trans Canada Highway & Shaganappi Trail NW Interchange**

**Policy T13.** The Montgomery Community Association and area residents should be consulted on any future interchange proposals for the Trans Canada Highway and Shaganappi Trail (including approach roads). Local concerns including sound mitigation, gateway enhancements, tree planting and pedestrian connections should be considered and where appropriate, incorporated into the final design.

**Policy T14.** Future transportation improvements at the Trans Canada Highway and Shaganappi Trail interchange that provide commuters with an alternate route to shortcutting through Montgomery are supported by this Plan.

### **32 Avenue & Shaganappi Trail**

**Policy T15.** The open space area at the intersection of Shaganappi Trail and 32 Avenue NW will be required for future transportation purposes. At such time that new roads are being designed, The City of Calgary should consult with area residents and the Montgomery Community Association on the development of detailed plans.





*Bowmont Office of Community and Neighbourhood Services.*



## CONTEXT

- 6.1 The demographic make up of a community provides an indication of its requirements for community programming and social supports. Social issues present in Montgomery include an above average proportion of single-parent and low-income households. In addition, Montgomery has a significant number of seniors and aboriginal people with unique needs for supports and services (refer to 5. Community Demographics in Section 3).
- 6.2 The following social issues within Montgomery were identified during the preparation of the ARP.
- Current services for seniors in Montgomery are insufficient to meet demand, particularly services for lower-income seniors.
  - Currently there are no direct services to meet the needs of the Aboriginal population.
  - There is a lack of community services provided within Montgomery. Existing services for Montgomery residents are provided outside of Montgomery, making access difficult, particularly, youth, day-care and employment resource services.
  - Many community programs and services in Montgomery rely on volunteers. However, the recruitment of volunteers is becoming increasingly difficult, challenging the ongoing sustainability of those programs and services.
  - Existing community facilities and amenity space are difficult for residents to access as it requires them to cross Bowness Road and 16<sup>th</sup> Ave. This is a particular concern for children and seniors.
  - By-law infractions related to waste reduce community pride.

## **SOCIAL DEVELOPMENT OBJECTIVES**

- (a) Identify community needs for social development and address them through a range of support programs and services. Specifically, assess and address issues faced by single-parent families, youth, seniors and Aboriginal persons.
- (b) Promote community well-being by supporting the development of community-based, affordable and accessible services and self-help initiatives.
- (c) Encourage public and private agencies and community based groups involved in service delivery to share the use of facilities where possible in order to provide needed services within Montgomery.
- (d) Support efforts to enhance community volunteer capacity.

Several actions are recommended by the ARP to achieve the above social development objectives (refer to Section 2: Redesignations.& Actions Table 6.7).

# SECTION 2: REDESIGNATIONS & ACTIONS

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## SECTION 2: REDESIGNATIONS & ACTIONS

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Transportation .....	A15
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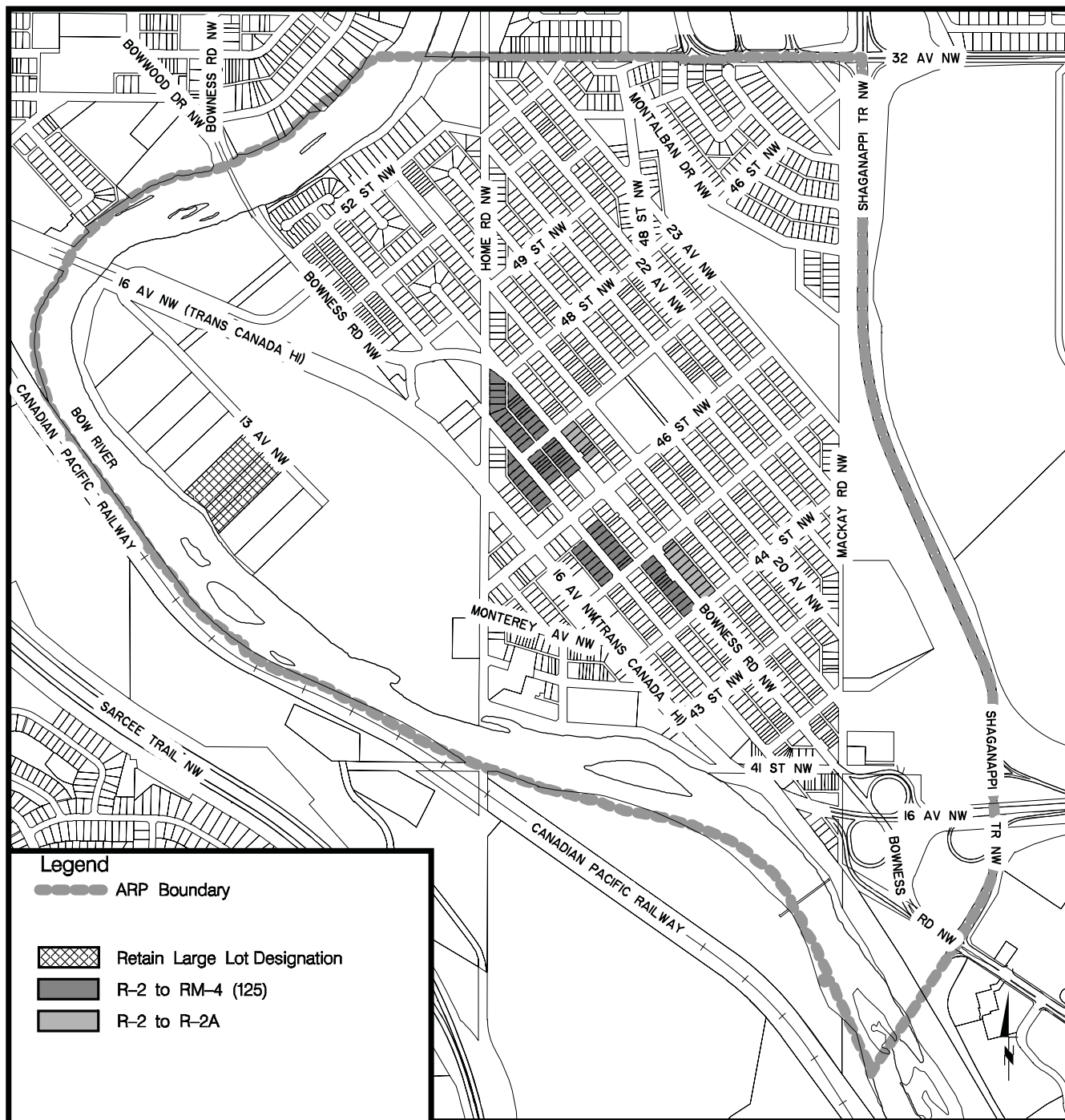
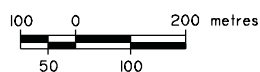


FIGURE 6.1

## RESIDENTIAL REDESIGNATIONS



Date Plotted: 05-OCT-2005

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**Table 6.1 Residential Redesignations & Actions**

<b>Site Address</b>	<b>2004 Land Use Designation</b>	<b>Redesignation Action</b>
5001, 5011, 5023 - 13 Avenue NW	DC (819) Country Residential	<ul style="list-style-type: none"> <li>• Retain large lot designation.</li> </ul>
4503, 4507, 4511, 4515, 4519, 4521, 4523, 4525, 4527, 4531, 4535, 4539, 4719, 4723, 4725, 4727, 4729, 4731, 4735, 4737, 4739, 4803, 4807, 4811, 4815, 4817, 4819, 4823, 4825, 4827, 4831 Bowness Road NW  4602, 4604, 4608, 4612, 4616, 4620, 4624, 4628, 4632, 4603, 4607, 4611, 4615, 4619, 4623, 4627, 4724, 4728, 4730, 4732, 4734, 4736, 4740, 4703, 4707, 4711, 4715, 4719, 4723, 4727, 4731, 4735, 4804, 4808, 4812, 4818, 4820, 4824, 4828 - 17 Avenue NW  1804, 1808, 1812, 1816, 1820 Home Road NW	R-2	<ul style="list-style-type: none"> <li>• Redesignate to RM-4(125).</li> </ul>
4504, 4508, 4512, 4516, 4520, 4524, 4528, 4532, 4536, 4540, 4542, 4720, 4722, 4724, 4728, 4732, 4736, 4740 Bowness Road NW	R-2	<ul style="list-style-type: none"> <li>• Redesignate to R-2A</li> </ul>

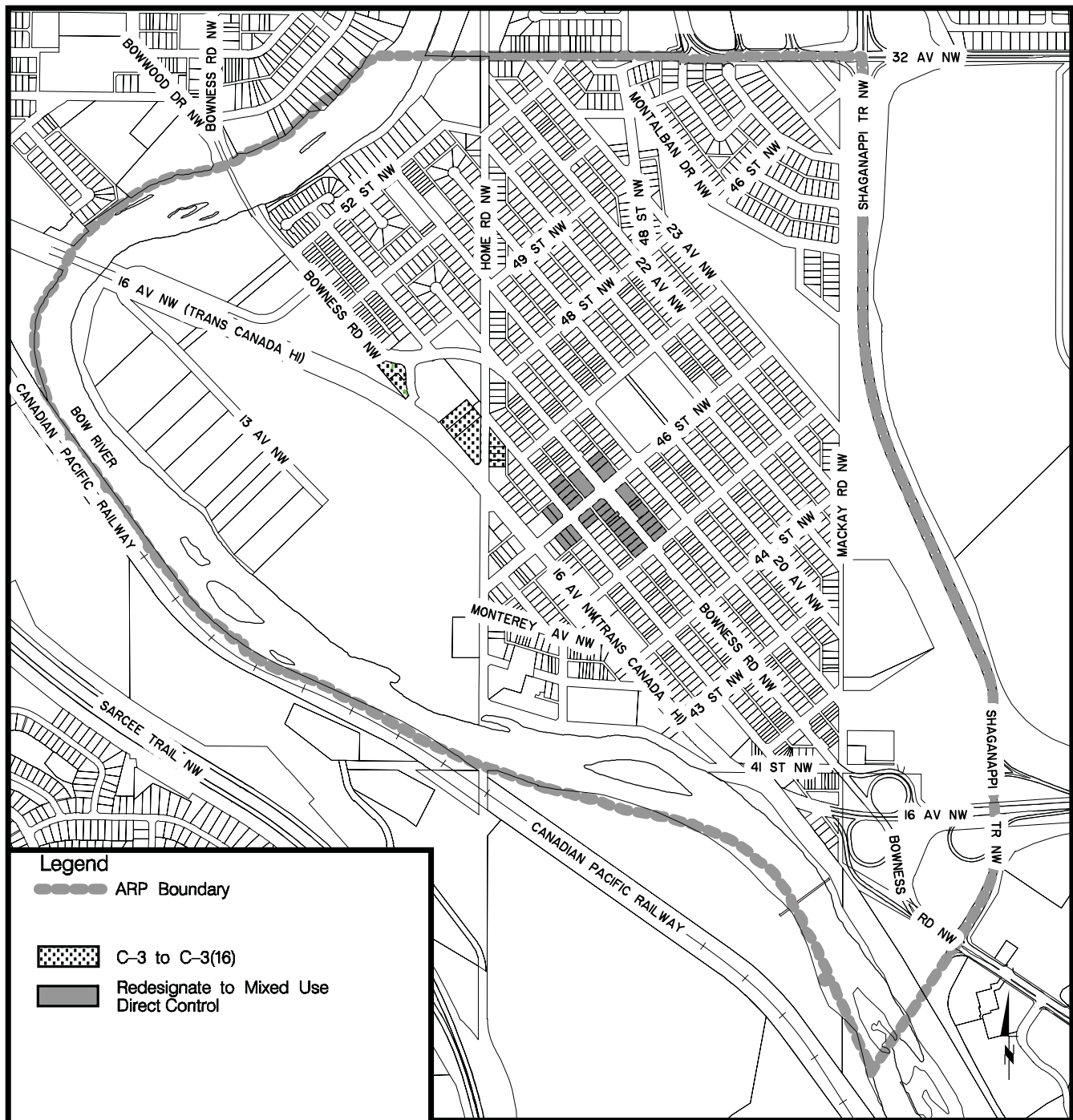
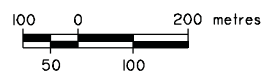


FIGURE 6.2

## COMMERCIAL REDESIGNATIONS AND ACTIONS



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**Table 6.2 Commercial Redesignations  
(Refer to Figure 6.2)**

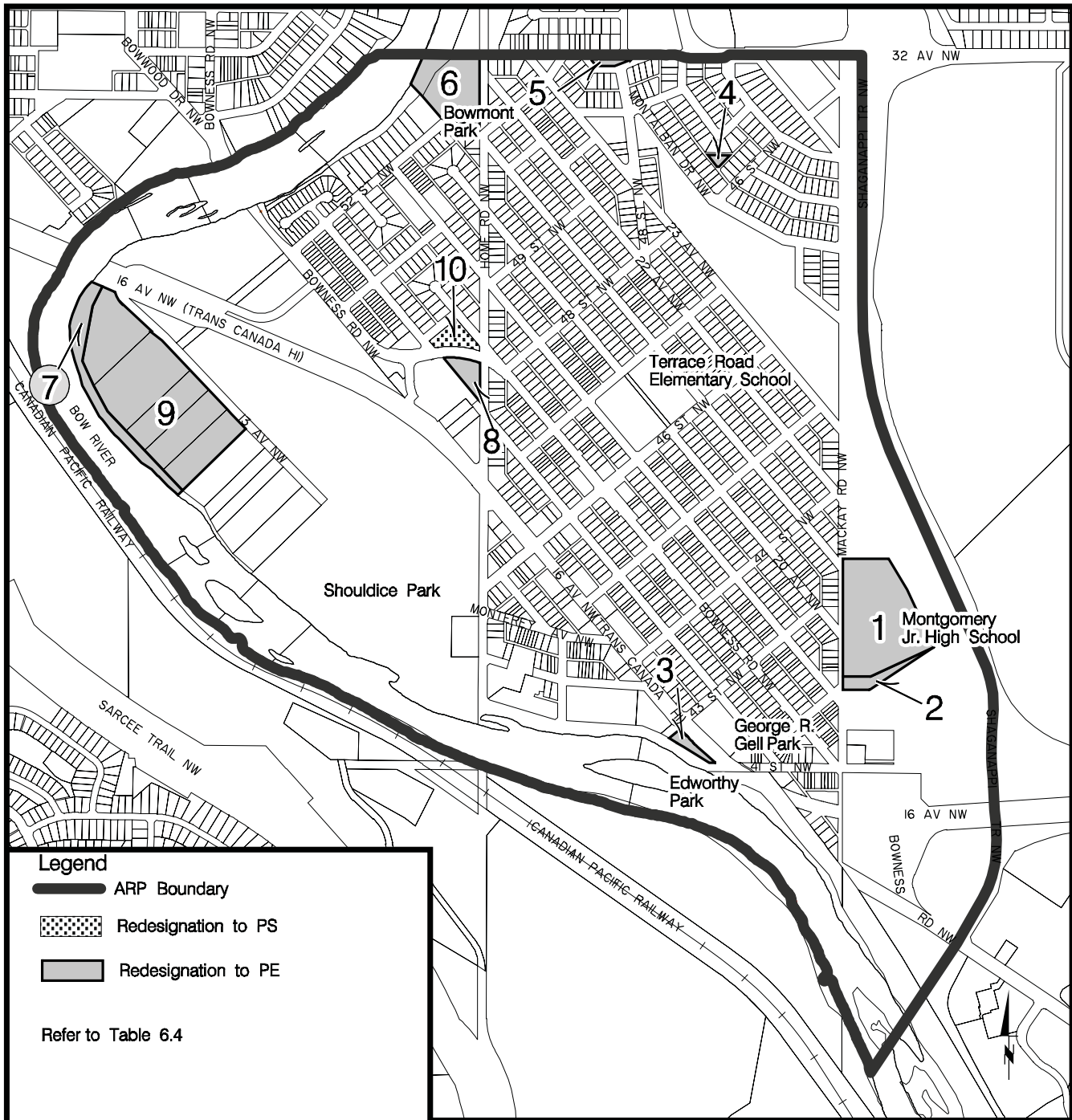
Site Address	2004 Land Use Designation	Redesignation
4604, 4608, 4612, 4614, 4620, 4624, 4626, 4640 Bowness Road NW  4636, 4640, 4704 - 17 Avenue NW  1712, 1718, 1715, 1719 - 46 Street NW	C-3	<ul style="list-style-type: none"> <li>• Redesignate to Mixed Use Direct Control District based upon the C-3 District. Guidelines should include:               <ul style="list-style-type: none"> <li>- height limit of three storeys and 12 metres to the eave and 15 metres to the peak of the roofline;</li> <li>- front yard set back minimum of 1.5 metres from the basic right-of-way;</li> <li>- remove the following uses from C-3 - Automotive Sales, Automotive Rental, Autobody and Paint Shops, Automotive Specialities and Car Washes;</li> <li>- Grade level should be used for commercial use;</li> <li>- Commercial laundry facilities excluded.</li> </ul> </li> </ul>
4603, 4607, 4611, 4615, 4619, 4623, 4627, 4629, 4631, 4635, 4639 Bowness Road NW	C-3	<ul style="list-style-type: none"> <li>• Redesignate to Mixed Use Direct Control District based upon the C-3 District. Guidelines should include:               <ul style="list-style-type: none"> <li>- height limit of four storeys and 15 metres to the peak of the roofline;</li> <li>- front yard set back minimum of 1.5 metres from the basic right-of-way;</li> <li>- remove the following uses from C-3 - Automotive Sales, Automotive Rental, Autobody and Paint Shops, Automotive Specialities and Car Washes;</li> <li>- Grade level should be used for commercial use;</li> <li>- Commercial laundry facilities excluded.</li> </ul> </li> </ul>
4704 Bowness Road NW	DC 819	<ul style="list-style-type: none"> <li>• Redesignate to Mixed Use Direct Control District based on the C-3 District as above.</li> </ul>
4712 & 4716 Bowness Road NW	DC 187Z81	<ul style="list-style-type: none"> <li>• Redesignate to Mixed Use Direct Control District based on the C3 District as above.</li> </ul>
4703 Bowness Road NW	C-1	<ul style="list-style-type: none"> <li>• Redesignate to Mixed Use Direct Control District based on the C3 District as above.</li> </ul>

**Table 6.2 Commercial Redesignations Continued...**

Site Address	2004 Land Use Designation	Redesignation
4720, 4716, 4712, 4708 - 17 Avenue NW	DC 129Z97	<ul style="list-style-type: none"> <li>• Redesignate to mixed use or residential Direct Control District.</li> </ul> <p>Guidelines should include:</p> <ul style="list-style-type: none"> <li>• All levels restricted to residential use</li> <li>- except grade which has the option for the following discretionary commercial uses only. Personal service business, financial institutions (CU), grocery stores and liquor stores;</li> <li>height limit of three storeys and 12</li> <li>- metres to the eave and 15 m to the peak of roofline for mixed use;</li> <li>height limitation to three storeys and 10</li> <li>- metres to the eave and 13 m to the peak of the roofline for residential only;</li> <li>front yard setback minimum of 1.5</li> <li>- metres from the basic right-of-way;</li> <li>- commercial laundry facilities excluded.</li> </ul>
1708, 1712, 1716, 1720 Home Road NW 5012, 5020, 5032 - 16 Avenue NW 1701 49 Street NW 5111 Bowness Road NW	C-3	<ul style="list-style-type: none"> <li>• Redesignate to C-3(16).</li> </ul>

**Table 6.3 Commercial Actions**

Location	Action
Bowness Road/46 Street Commercial Area	<ul style="list-style-type: none"><li>• The Community Association and The City of Calgary to encourage businesses to set up a Business Revitalization Zone for marketing and promotions.</li><li>• Community Association, property owners, businesses and The City of Calgary should work cooperatively to seek funding for improvement to the streetscape environment.</li></ul>



**Table 6.4 Parks & Institutional Redesignations  
(Refer to Figure 6.3)**

Site	Site Address	2004 Land Use Designation	Redesignation
1.	Montgomery Junior High School 2116 Mackay Road NW	DC (819) for educational, recreation & Community services	• Redesignate to PE
2.	Baseball Diamond 2004 Mackay Road NW	DC (819) for educational, recreation & community services	• Redesignate to PE
3.	Community Entry 4325 - 16 Avenue NW	R-2	• Redesignate to PE
4.	Tot Lot 3001 - 46 Street NW	R-1	• Redesignate to PE
5.	32 Avenue Buffer 5214 Sheldon Place NW	DC (819) for educational, recreation & community services	• Redesignate to PE
6.	Bowmont Park Entrance 2123 - 52 Street NW	DC (819) for educational, recreation & community services	• Redesignate to PE
7.	Riverbank	DC (47Z87) for Semi-detached dwellings	• Redesignate to PE
8.	Trail & Bench Park (Safeway's) 5007 Bowness Road NW	DC (819) for educational, recreation & community services	• Redesignate to PE
9.	Shouldice Park 5111, 5123, 5203, 5219, 5227 - 13 Avenue NW	DC (819) for educational, recreation & community services & Country Residential and DC (62Z92) for townhouses & apartments	• Redesignate to PE
10.	EMS 5000 Bowness Road NW	DC (819) for educational, recreation & community services	• Redesignate to PS

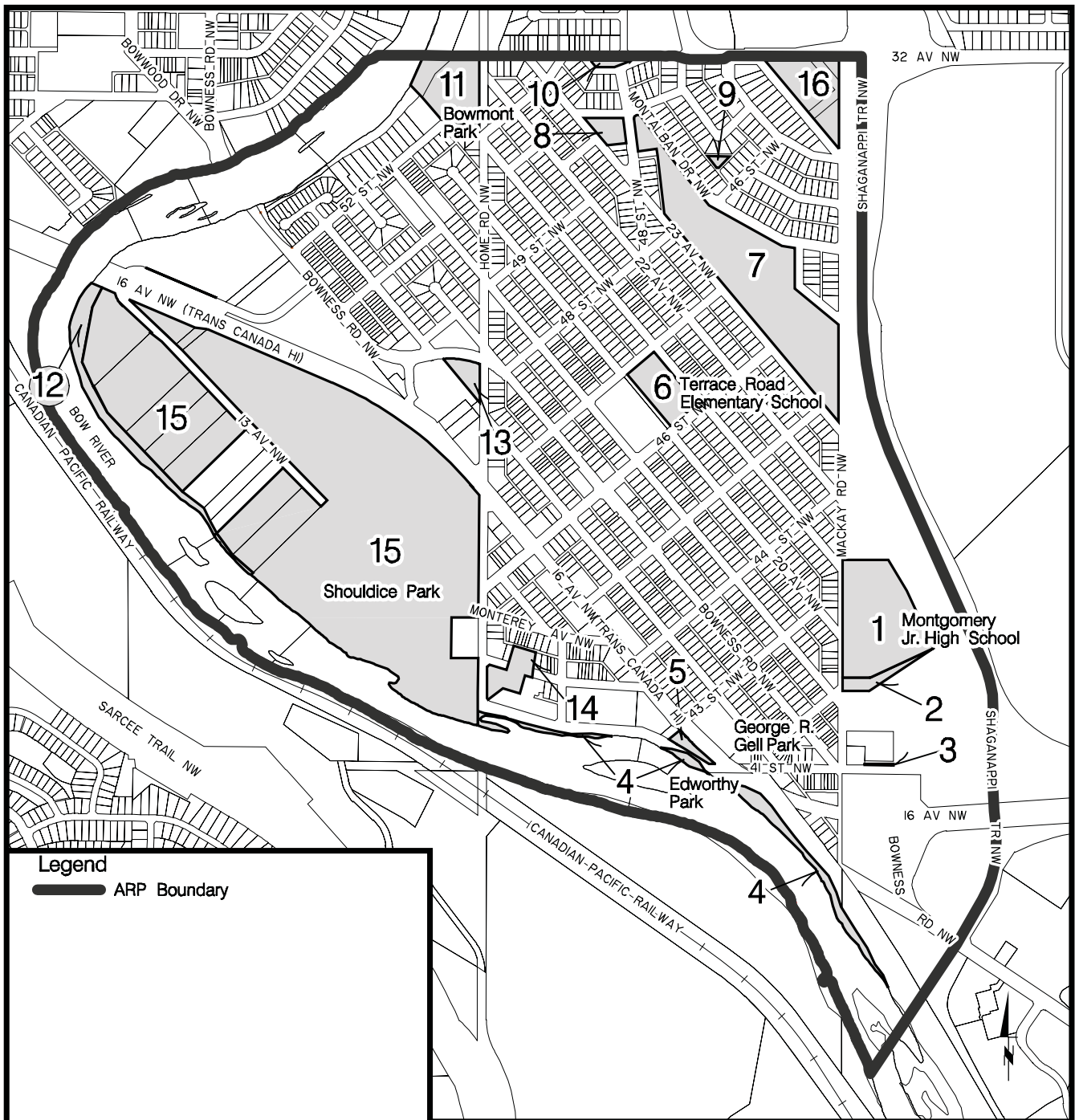


FIGURE 6.4

## PARKS AND INSTITUTIONAL ACTIONS

**Table 6.5 Parks and Institutional Actions**

Site	Site Description	Location	Action	Responsibility
1.	Montgomery Junior High School	3115 Mackay Road NW	<ul style="list-style-type: none"> <li>Retain as local park space if school is closed.</li> </ul>	City of Calgary, Calgary School Board and the Community Association to work co-operatively.
2.	Baseball Diamond	2004 Mackay Road NW	<ul style="list-style-type: none"> <li>Name the park.</li> </ul>	See * below.
3.	Community Entry Trees	1902 R Mackay road NW	<ul style="list-style-type: none"> <li>Plant trees when funds are available.</li> </ul>	City of Calgary.
4.	Riverbank - east	Pt. of 4106 - 16 Avenue NW	<ul style="list-style-type: none"> <li>Preserve and protect.</li> </ul>	City of Calgary.
5.	Community Entry	4325 - 16 Avenue NW	<ul style="list-style-type: none"> <li>Plant trees when funds are available.</li> </ul>	City of Calgary.
6.	Terrace Road Elementary School	2103 - 46 Street NW	<ul style="list-style-type: none"> <li>Retain as local park if school is closed.</li> <li>Upgrade the playground.</li> </ul>	City of Calgary, Calgary School Board and the Community Association to work co-operatively.
7.	Montalban Park/ Montgomery Hill/ Montgomery Terrace	4707 Montalban Drive NW	<ul style="list-style-type: none"> <li>Name the park.</li> <li>Upgrade with trees, grass and provide garbage cans when funds are available.</li> </ul>	See * below. City of Calgary.

\* Community Association to organize selection of name for the park and submit to the Accountability Priorities and Agenda Committee (APAC) for consideration. If approved by City Council signage funding should be sought from the Calgary Parks Budget.

**Table 6.5 Parks and Institutional Actions Continued...**

Site	Site Description	Location	Action	Responsibility
8.	Tot Lot	5215 Montalban Avenue NW	• Name the park.	See * below.
9.	Tot Lot	3001 - 46 Street NW	• Name the park.	See * below.
10.	32 Avenue Buffer	5214 Sheldon Place NW	• Name the park.	See * below.
11.	Bowmont Park Entrance	2123 - 52 Street NW	• Name the park. Enhance flat area.	See * below.
			• Heritage interpretation opportunity.	Community Association.
			• Improve parking lot drainage when funds are available.	City of Calgary.
			• Evaluate area from CPTED principles.	City of Calgary.
			• Improve lighting and security when funds are available.	City of Calgary.
12.	Riverbank - west	Pt. of 5802 Bowness Road NW, 60R and 76R Bow Landing NW  Pt. of 5111 - 5227 - 13 Avenue NW	• Preserve & protect.	City of Calgary.
13.	Trail & Bench Park (Safeway's)	5007 Bowness Road NW	• Determine use. • Name the park.	City of Calgary. See * below.
14.	Bow Manor Park	1308 Home Road NW	• Name the park.	See * below

\* Community Association to organize selection of name for the park and submit to the Accountability Priorities and Agenda Committee (APAC) for consideration. If approved by City Council signage funding should be sought from the Calgary Parks budget.

**Table 6.5 Parks and Institutional Actions Continued...**

Site	Site Description	Location	Action	Responsibilities
15.	Shouldice Park	1515 Home Road NW  4900, 4911, 4923 - 13 Avenue NW Pt. of 5299 Bowness Road NW  5123, 5219, 5227 - 13 Avenue	<ul style="list-style-type: none"> <li>• Complete Master Plan initiated in 2002.</li> <li>• Improve signage to the Park along 16 Avenue NW and Home Road NW when funds are available.</li> <li>• Improve passive recreational amenities including more park benches when funds are available.</li> </ul>	City of Calgary.
16.	Shaganappi Trail Road Widening	3225 Mackay NW	<ul style="list-style-type: none"> <li>• Retain for future transportation services.</li> <li>• Retain Tot Lot until land is required for transportation upgrades.</li> </ul>	City of Calgary.

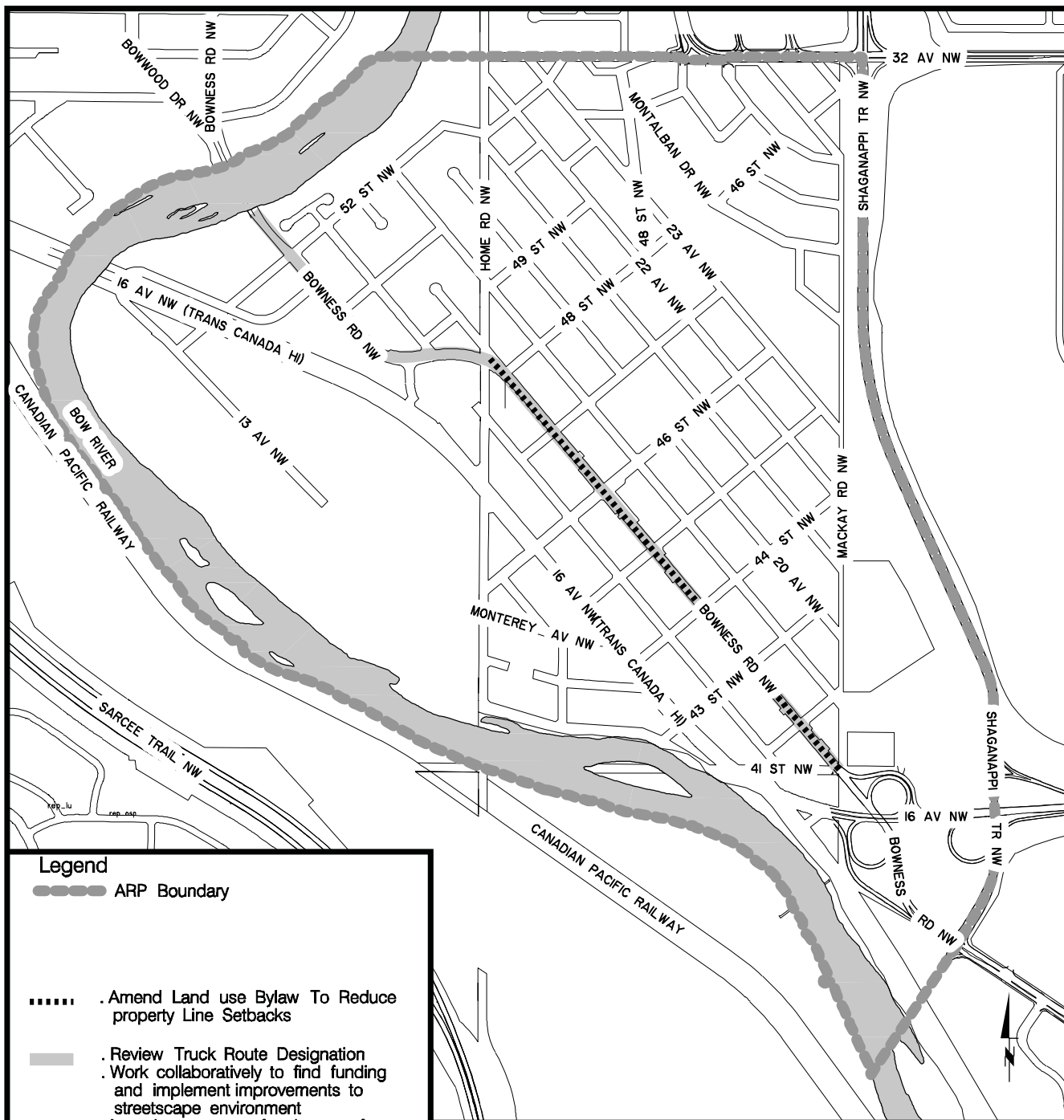
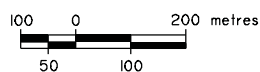


FIGURE 6.5

## TRANSPORTATION ACTIONS



Date Plotted: 14-APR-2004



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**Table 6.6 Transportation Actions**

Description/Location	Action
Bowness Road NW from 51 Street NW to Mackay Road NW	<ul style="list-style-type: none"> <li>The City of Calgary to amend the <i>Land Use Bylaw Part 3, Division 1</i> to reduce the property line setbacks from 5.182 metres to 1.5 metres on each side. <b>Bylaw 38P2008</b></li> <li>The City of Calgary in conjunction with area land owners, business operators and the Community Association should work cooperatively to seek funding for improvements to the streetscape environment of Bowness Road.</li> </ul>
Bowness Road	<ul style="list-style-type: none"> <li>The City of Calgary in conjunction with area land owners should review the potential to remove the truck route designation on Bowness Road.</li> <li>The Community Association in conjunction with land owners on Bowness Road should investigate the level of local area support to change the name of Bowness Road to one that reflects the character and image of the community of Montgomery.</li> </ul>
Western side of Bowness Road fronting 4500 Block	<ul style="list-style-type: none"> <li>Currently there is no sidewalk fronting the properties on this stretch of Bowness Road. The City of Calgary in conjunction with property owners and the Community Association should work cooperatively to seek funding for sidewalk provision.</li> </ul>
Home Road & 32 Avenue NW	<ul style="list-style-type: none"> <li>The City of Calgary to examine the need for flashing pedestrian crosswalk through the Standard Warrant Evaluation Procedure.</li> </ul>
43 Street NW & Bowness Road NW	<ul style="list-style-type: none"> <li>The City of Calgary to examine the need for flashing pedestrian crosswalk through the Standard Warrant Evaluation Procedure.</li> </ul>
Pedestrian links to and from community focal points (Refer to Figure 5.4)	<ul style="list-style-type: none"> <li>Property owners, the Community Association and The City of Calgary are encouraged to work cooperatively to achieve a walkable neighbourhood.</li> </ul> <p>Potential improvements include sidewalk provision seating benches (particularly along hilly routes), tree planting, pedestrian oriented signage, safety improvements at cross walks and traffic calming initiatives.</p>

**Table 6.7 Social Development Actions**

<b>Actions</b>	<b>Responsibility</b>
Work with other service providers to assess needs and coordinate and enhance access to services and resources for seniors, children, and youth within Montgomery.	City of Calgary, Community Services Department.
Work with aboriginal, public and private groups to assess the needs of the aboriginal population within Montgomery and develop strategies to address such needs.	City of Calgary, Community Services Department.
Where programs and services are identified as needed, encourage the provision of community-based service where feasible and appropriate.	City of Calgary, Community Services Department.
Work with public and private groups to promote opportunities for community volunteering.	City of Calgary, Community Services Department.
Work with public and private groups to develop and promote community clean-up activities.	City of Calgary, Community Services Department and Protective Services Department.

# SECTION 3: BACKGROUND INFORMATION

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## SECTION 3: BACKGROUND INFORMATION\*

### Contents

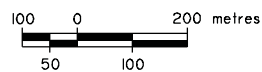
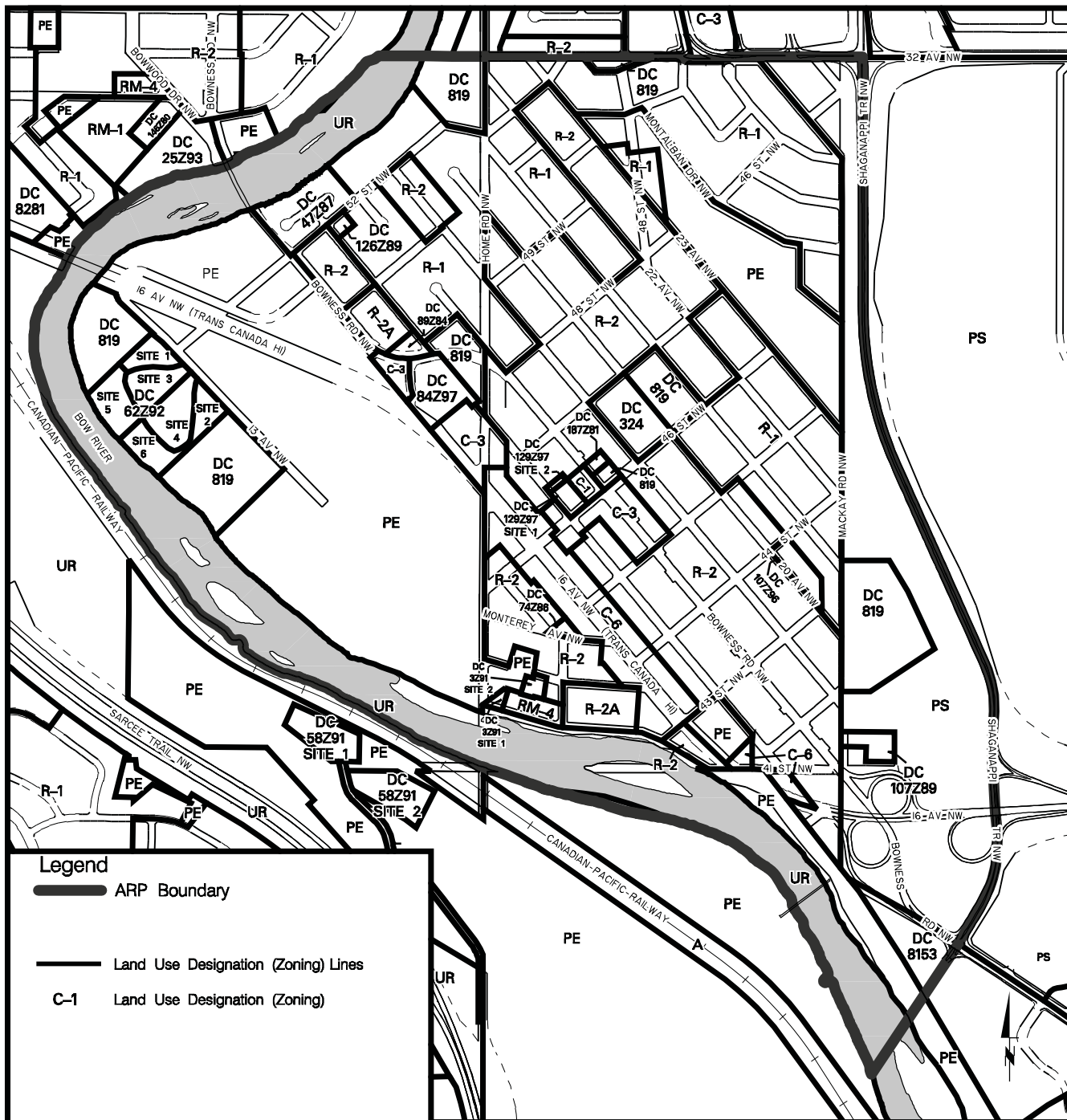
1.0 Existing Land Use Designations .....	B3
2.0 Future Land Use Plan Insets .....	B5
3.0 Relevant Planning Policies .....	B9
4.0 Community History & Geography .....	B15
5.0 Community Demographics .....	B19
6.0 Residential Background Information.....	B25
7.0 Commercial Background Information .....	B33
8.0 Transportation Background Information.....	B39

\* Section 3 - Background Information Section contains supporting information, relevant research and community perspective and is not part of the ARP Bylaw.



# 1.0

# EXISTING LAND USE DESIGNATIONS



Date Plotted: 14-APR-2004

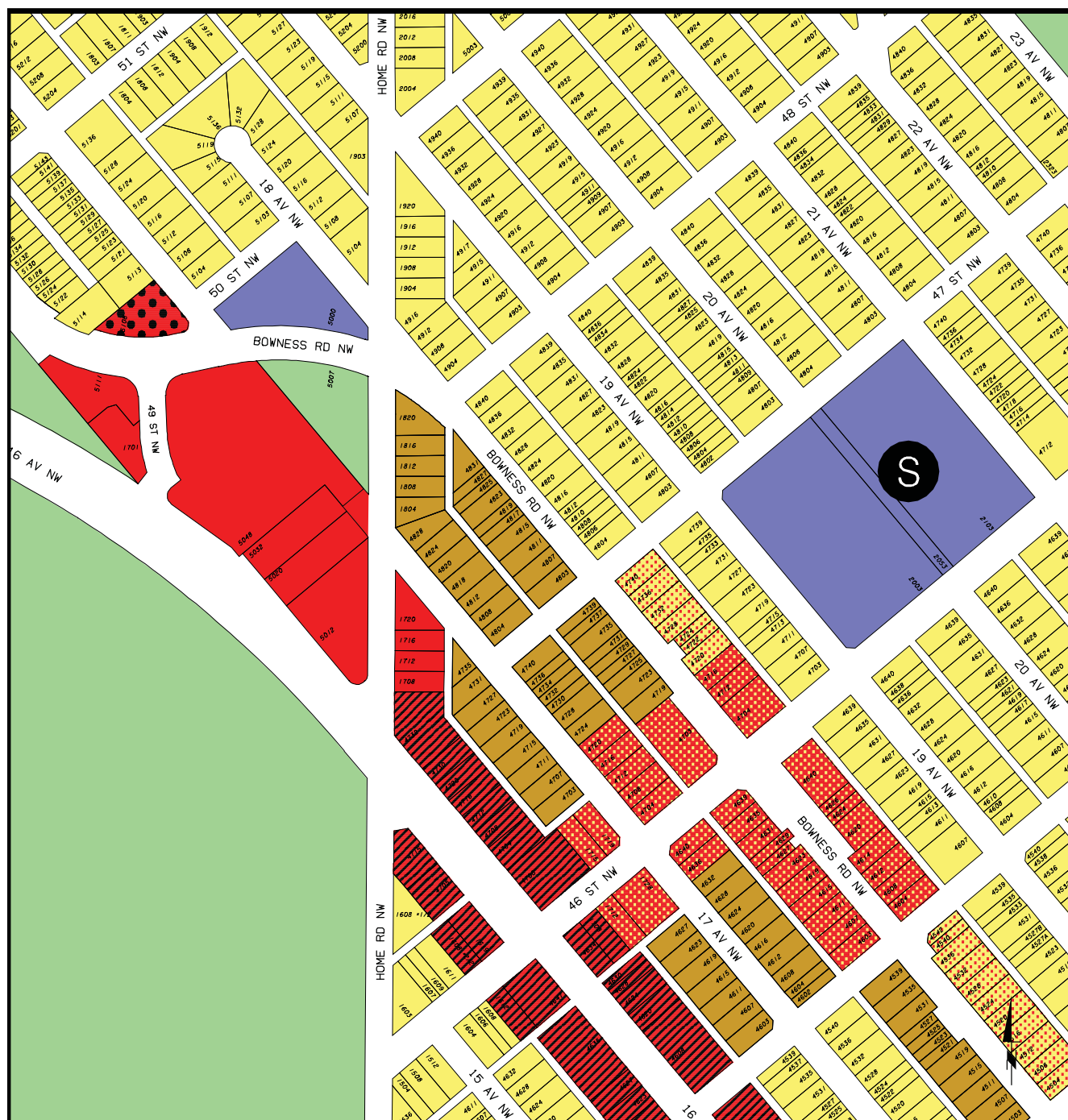


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## FUTURE LAND USE PLAN INSETS

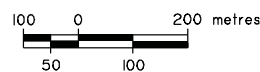


### Inset 1

### Legend

- 
- |   |  |   |                                   |
|---|--|---|-----------------------------------|
|  | Low Density Residential                  |  | Institutional\Schools             |
|  | Low Density Residential\Townhouse        |  | Utilities                         |
|  | Medium Density Residential\Institutional |  | Large Lot Residential             |
|  | Medium Density Residential               |  | Future Transportation             |
|  | Mixed Use Commercial\Residential         |  | Future Transportation /Open Space |
|  | Local Commercial                         |  | Storage                           |
|  | General Commercial                       |   |                                   |
|  | Highway Commercial                       |   |                                   |
|  | Parks\Community Facilities               |   |                                   |
- 100  
50

## FUTURE LAND USE PLAN


















Date Plotted: 05-OCT-2005



Draw No.: MONT:inset1\_concept



## Legend

	Low Density Residential		Institutional/Schools
	Low Density Residential/Townhouse		Utilities
	Medium Density Residential/Institutional		Large Lot Residential
	Medium Density Residential		Future Transportation
	Mixed Use Commercial/Residential		Future Transportation /Open Space
	Local Commercial		Storage
	General Commercial		
	Highway Commercial		
	Parks/Community Facilities		

Inset 2

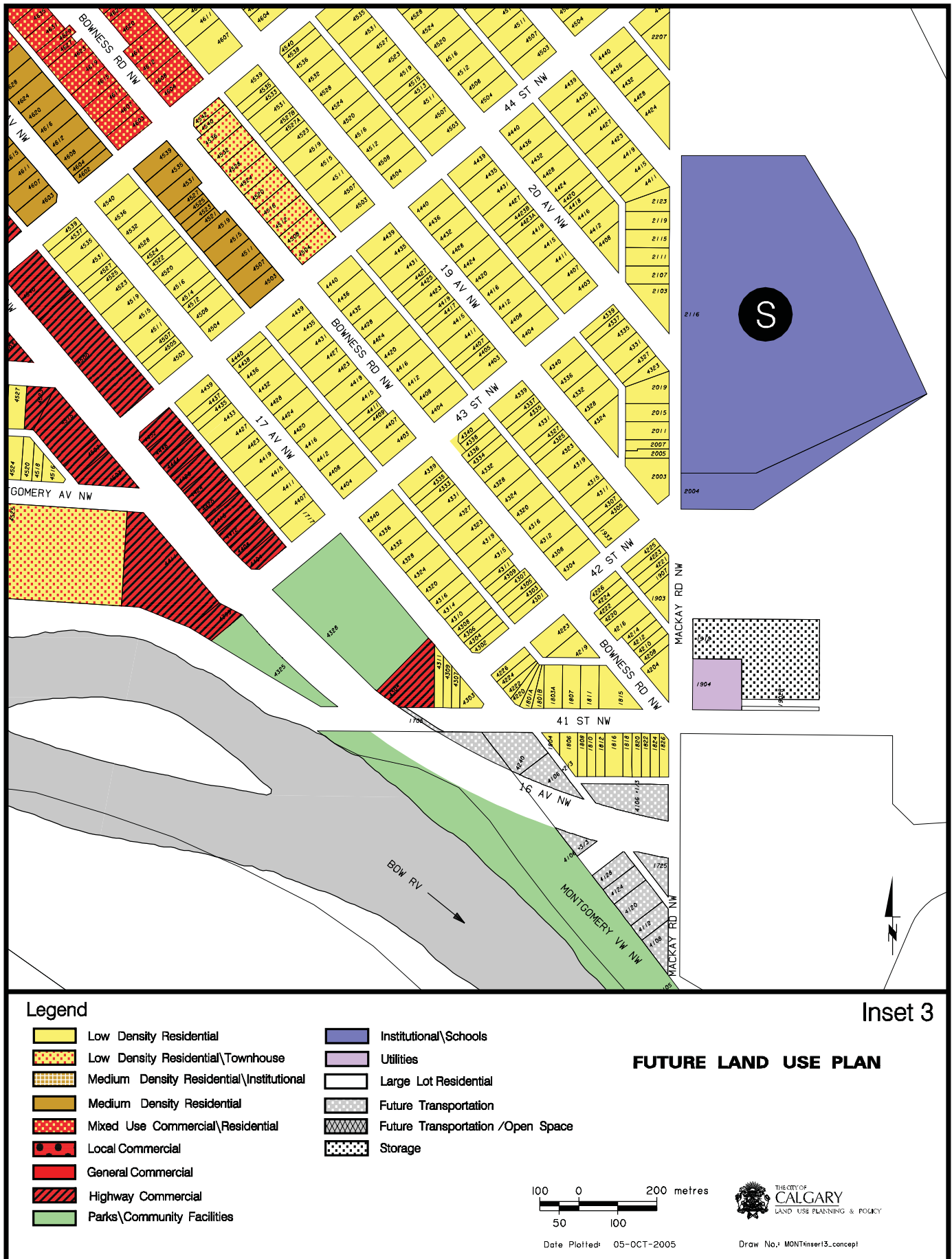
## FUTURE LAND USE PLAN



Date Plotted: 05-OCT-2005



Draw No.: MONTInset2-concept





## **3.0 RELEVANT PLANNING POLICIES**

### **3.1 Calgary Land Use Bylaw 2P80 – Adopted by Council 1980**

This Bylaw is the basic land use control document that provides the specific regulatory rules and guidelines that govern land use, development and subdivision within Montgomery and all other parts of Calgary.

### **3.2 The Municipal Development Plan (The Calgary Plan) – Adopted by Council 1998**

*The Calgary Plan* is the pre-eminent statutory plan guiding growth and development within Calgary. *The Municipal Government Act* requires that all statutory plans are consistent with one another and therefore *the Calgary Plan* provides the strategic citywide framework guiding local level plans and policies.

*The Calgary Plan* identifies a series of policies to achieve a long-term vision and address the challenges that come with growth and change. There are numerous objectives and policies in *the Calgary Plan* that are relevant to the preparation of Montgomery's Area Redevelopment Plan. Three major policy themes can be summarized as:

- (a) Maintaining and improving the health of the physical environment.
- (b) Managing growth by focusing on the efficient use of land and the optimization of the transportation system.
- (c) Recognizing the importance of the health of communities and acknowledging that social, community and economic factors contribute towards maintaining and improving quality of life.

### **3.3 Calgary Transportation Plan (Go-Plan) – Approved by Council 1995**

The *Go-Plan* is a 30-year strategic blueprint that guides transportation planning to a city population size of 1.25 million people. The *Go-Plan* identified that established neighbourhoods should be capable of accommodating higher average densities to support greater housing choices, adaptability and the efficient use of public services. Key relevant policies include:

- (a) Growth will be directed to locations that lessen the need for new river crossings in the future.

- (b) Encourage sensitive types of housing intensification in all neighbourhoods, in accordance with local plans, to promote a more compact, adaptable form.
- (c) Encourage new housing close to transit facilities and within mixed-use centres to support transit and pedestrian mobility choices.
- (d) Locate compatible jobs within and close to residential neighbourhoods to support walking, cycling and shorter vehicle trips.

### **3.4 Sustainable Suburbs Study – Adopted by Council 1995**

This study seeks to encourage developers, City departments and others to find new ways of designing more sustainable communities. There is no intent to impose any single design approach. The recommended strategy is to design communities along the lines of an urban village. An adequate choice of shops and services should be provided locally so that residents are not dependent on regional shopping centres for most daily needs and local business and employment is encouraged. The design focus is on improving the public realm, making communities more attractive and liveable for people of all ages and lifestyles, while significantly reducing the need for many vehicle trips.

The following are the major elements common to the design of more sustainable communities:

- (a) A focal point and recognizable boundaries and entrances that give the community a distinct identity.
- (b) A public activity centre, offering a variety of goods and services sufficient to meet people's daily needs.
- (c) A mixture of residential, public and commercial uses at and near the activity centre.
- (d) Parks, schools and shops within a comfortable walking distance of homes.
- (e) Safe, pedestrian and cyclist-friendly streets providing direct connections from homes to community and transit facilities.
- (f) A wide choice of housing types and costs to meet a variety of household types and lifestyles.

- (g) A range of local employment opportunities.
- (h) An efficient and effective public transit system that provides a viable option to the car, especially for the journey to work.
- (i) Protected natural areas and a variety of linked open spaces offering a choice of activities, connected where possible to the regional open space system.
- (j) Connections to the regional pathway system providing a safe transportation and recreation option for pedestrians and cyclists.

### **3.5 Calgary Urban Park Master Plan – Adopted by Council 1994**

General:

- Extension of Shaganappi Trail across the Bow River not supported.
- Landscape features contributing to the visual continuity and aesthetic quality of the River Valley Park Systems will be protected and enhanced.

Shouldice Park:

- Park continues to operate as a premier athletic park hosting soccer, softball and football.
- Park should also provide public space adjacent to the River for more passive activities.
- Private properties adjacent to the Bow River within Shouldice Park should be acquired in cooperation with owners.
- Expand the naturalized area at the edge of the Bow River.
- Extend the regional pathway system.
- River access for non-motorized river craft to be considered south of the Trans-Canada bridge.

Bowmont Park:

- Acquisition and reclamation of the existing gravel extraction area is proposed. Where arrangements with private landowners cannot be made, the legal rights of the landowner shall be respected.
- Automobiles will be kept to the periphery of the Park.

### **3.6 Improving Calgary's Entranceways – Approved By Council 1994**

The entranceway study recommended improvements to Calgary's entranceways to ensure that visitors arriving by road or by air form a favourable first impression of the city. The Trans Canada Highway (16<sup>th</sup> Avenue NW) as it runs through Montgomery was identified as one of the areas forming the image of Calgary.

### **3.7 Low Density Residential Housing Guidelines For Established Communities – Approved By Council 1993**

Provide guidelines for low-density residential infills in Calgary's established communities. Montgomery is considered an established community. The Guidelines foster a high standard of design and development and encourage respect for the community context and the streetscape. The Guidelines deal with five main design elements:

- (a) Site context;
- (b) Site layout and parking;
- (c) Building mass (envelope);
- (d) Privacy and shadowing;
- (e) Landscaping.

### **3.8 Long Term Growth Management Strategy – Approved By Council 1986**

In established residential areas, The City will endeavour to optimize the use of existing servicing systems. Through the local planning process, the opportunities for

accommodating population increases will be identified in each community, ensuring that population increases will occur in ways which:

- (a) strengthen the role of the community within the built-up area, as defined in local area plans;
- (b) contribute to the community's quality and image;
- (c) contribute to the existing community fabric and social environments.

A variety of housing types, to serve the broadest spectrum of housing needs, should be encouraged within the built-up area. There should be a choice of housing types and living environments so as to provide for various types of populations in the existing communities, ranging from unattached persons, couples in their family formation years, middle-aged and older families. This does not mean that every community district is obliged to provide mix of housing. Rather, the appropriate mix in any given community district is to be determined through the local planning process.

### **3.9 Calgary River Valleys Plan – Adopted by Council 1984**

The River Valleys Plan identified a range of objectives and policies relating to environmental protection, flood mitigation, recreation and land use. Specific to Montgomery, the Plan identified the following specific policy directions:

- (a) Private lands adjacent to the Bow River and Shouldice Park identified as appropriate for open space/environmental reserve dedication. Low priority for land acquisition.
- (b) Bow River near the Trans Canada Highway right of way is appropriate for canoe launch.

### **3.10 Bowmont Design Brief – Adopted By Council 1978**

Facilitated land use and transportation changes for the communities of Montgomery and Bowness. The Design Brief outlined general residential land use reclassifications to R-1 in order to enhance residential stability and designated R-2 in those areas in need of rehabilitation and general re-development. *The Bowmont Design Brief* will be superseded by the *Montgomery Area Redevelopment Plan*.

### **3.11**

#### **The Transit Friendly Design Guide – Approved by Council 1995**

This guide describes techniques for improved integration of transit into residential and non-residential areas to achieve the vision described in the *Calgary Transportation Plan*. It is designed to explain and give examples of the physical requirements necessary for good transit service. The information will be useful to a broad range of stakeholders, including the public, elected officials, planners, communities, developers, the civic administration, engineers and architects.

## 4.0

## COMMUNITY HISTORY & GEOGRAPHY

### 4.1

### Community History

Montgomery's present character stems in part from its history as an independent municipality outside the boundaries of the city of Calgary. At the height of a property boom in 1910, local property owner and farmer James Shouldice envisioned Montgomery as an elite community that would rapidly become part of the greater Calgary area. James Shouldice dedicated the present Shouldice Park as a public park and agreed to grade a road to that location. In exchange The City agreed to extend the electrical street railway system to the proposed new residential area. The anticipated housing boom collapsed at the outset of WWI. It would not be until after the Great Depression and WWII, that development in Montgomery would significantly begin.



*James Shouldice  
(1850-1925)*

The town of Montgomery was annexed to the city of Calgary at its own request in 1963. According to the decision of the Local Authorities Board (No. 813, June 14<sup>th</sup>, 1963) the town of Montgomery was socially and economically tied to the city of Calgary. At that time, utility services were also connected to the City. Almost 90% of the town's assessment base was generated by residential development and there was no land within the town's boundaries for expanding the tax base with industrial development. The economic realities of the day suggest that annexation was the most logical solution for the town of Montgomery.



*Aerial view looking north at Montgomery 1957.*

In 1978 The City of Calgary adopted *The Bowmont Design Brief* providing direction for the redevelopment of Montgomery and Bowness. Prior to the adoption of the Design Brief, Council designated Montgomery as a community that would benefit from the Neighbourhood Improvement Program (N.I.P.) and the Residential Rehabilitation Assistance Program. Communities that qualified for the N.I.P program were mainly stable residential areas with lower to moderate incomes and significant portions of the housing stock in need of rehabilitation. In Montgomery the following projects were funded through the N.I.P program:

- (a) Shouldice Pool;
- (b) Montgomery Park improvements;
- (c) Montgomery Boys' Club;
- (d) Land purchase for community reserve;
- (e) Miscellaneous, street paving, sidewalk, curb and gutter.

In 1995 an Area Redevelopment Plan was adopted in Bowness and in 2000 the planning process was initiated to prepare an Area Redevelopment Plan for Montgomery.



*Shouldice Residence built 1911. Became the crematorium in 1937. Demolished 1972.*

## 4.2

### Montgomery Chronology of Events:

- 1910** Landowner and farmer James Shouldice dedicated Shouldice Park to The City of Calgary and secured streetcar service for the new community.
- 1914** Land-boom collapsed and development of Montgomery halts.
- 1947** 60 houses were built in Montgomery.
- 1955** Montgomery was designated as a Hamlet.
- 1963** Town of Montgomery annexed to Calgary at its request.
- 1963** Montgomery contained 1100 homes. Largest undeveloped area located in the easterly 1/3 of the community.
- 1976** Montgomery designated by City Council for Neighbourhood Improvement Program.
- 1978** *Bowmont Design Brief* adopted by Council.
- 1978** N.I.P projects initiated.
- 1979** Council initiated land use reclassifications conforming to the adopted *Bowmont Design Brief*.
- 1995** *Bowness Area Redevelopment Plan* adopted by Council.
- 2000** *Montgomery Area Redevelopment Plan* planning process initiated.
- 2002** *Shouldice Park Master Plan* initiated.



*Driving up Brickburn Hill (ca. 1912)*

### **4.3 Geography**

Montgomery is situated on the south-facing slope of the Bow Valley escarpment, to the north and east of the Bow River. The community's topography provides a diversity of slopes and landforms with excellent views of the Bow River valley. The natural features of the community including the steeply rising slopes, mature vegetation, excellent views, access to the Bow River and the escarpment areas make Montgomery an attractive residential environment. In addition to natural amenities, close access to downtown, shopping amenities and regional outdoor amenities also add to the community's attractiveness.

## 5.0 COMMUNITY DEMOGRAPHICS

### 5.1 Historic Population & Dwelling Units

The population of Montgomery declined by 26% from 1968-2003 with a net loss of 1,372 people. The most significant population losses occurred in the early 1970s, from 1971 to 1976 the population dropped by 18%. During the decade of the 1990s the population remained fairly stable, hovering around 4,000 people. The overall population loss experienced in Montgomery is typical of many older Calgary communities reflecting the lifecycle transition from families with children to empty nesters.

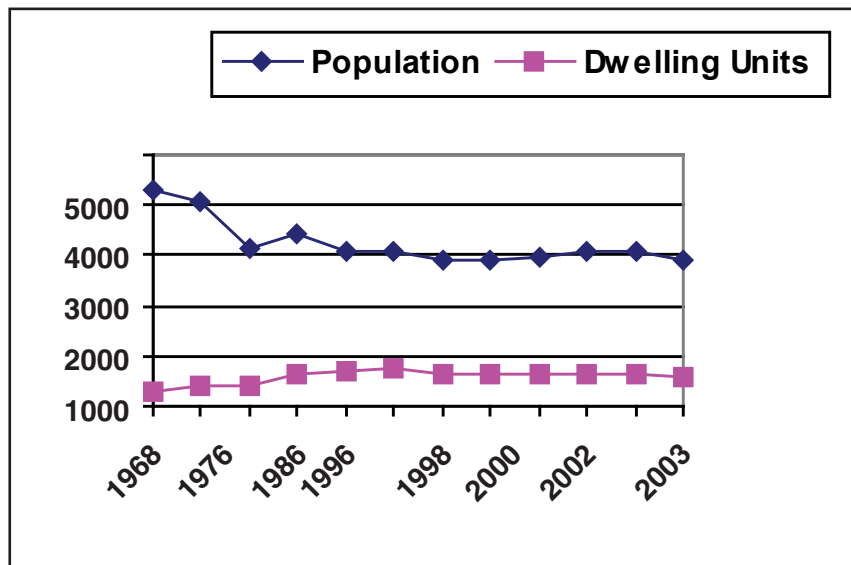
While the population declined from 1968-2003, the total number of dwelling units increased by almost 23% during the same time period. The most significant growth in units occurred between the late 1970s and the mid-1980s. During the 1990s (1990-2003), however, the number of dwelling units declined by 63 units or 4% and the population declined by 180 people or 4%.

**Table 1: Historic Population and Dwelling Units (1968–2001)**

Year	Population	% Change	Dwelling Units	% Change
1968	5280		1293	
1971	5067	-4%	1391	+7.55
1976	4136	-18%	1390	-.07%
1981	4407	+6.5%	1630	+17%
1986	4099	-.7%	1696	+4%
1990	4088	-.2%	1727	+1.8%
1996	3910	-4%	1662	-4%
1997	3903	-.17%	1638	-1.4%
1998	3971	+1.7%	1634	<1%
1999	4073	+2.5%	1689	<1%
2000	4057	-.4%	1651	<1%
2001	3909	-3.7%	1656	>1%
2002	4024	2.9%	1655	<1%
2003	3908	3%	1664	<1%

Source: Calgary Civic Census

**Chart 1: Historic Population & Dwelling Units Trends**



Source: Calgary Civic Census

## 5.2 Age Group Breakdown

Montgomery is very similar in age composition to the city of Calgary for ages 15-64. The community however contains an aging population and fewer children as compared to the rest of the city. Montgomery is under-represented in the 0-14 age group and over-represented by people 65 and over. The large number of seniors in the community can be explained in part by the existence of the Bow View Nursing Home and James Shouldice Lodge and by the age of the community.

## 5.3 School Enrollment

Enrollment in the Terrace Road Elementary School has declined 26% from 1990-2000. The Montgomery Junior High School, however, has experienced an 22% increase of 'home area' students from 1990-2000. The junior high school has a significant population of bused students whose enrollment has grown by 13% during the 1990s.

**Table 2: Age Group Break Down (2003)**

Age	Montgomery	Montgomery %	Calgary	Calgary %
0-4	207	5%	51,069	6%
5-14	362	9%	116,308	13%
15-19	190	5%	59,464	7%
20-24	314	8%	67,927	8%
25-34	710	18%	147,049	17%
35-44	713	19%	168,264	19%
45-54	479	12%	125,303	14%
55-64	261	7%	62,367	7%
65-74	402	10%	46,530	5%
75+	271	7%	32,238	4%
Total	3909	100%	876,519	100%

Source: Calgary Civic Census 2003

## 5.4 Occupancy Rates (people per household)

The average number of persons per household or the occupancy rate in Montgomery has decreased from 4.08 in 1968 to 2.47 in 2003. Declining occupancy rates are typical for older Calgary communities and reflect community life-cycle factors of children leaving the family home.

**Table 3: Historic Dwelling Unit Occupancy Rates (1968–2003)**

Year	Montgomery	Calgary
1968	4.08	3.29
1971	3.64	3.04
1976	2.97	2.78
1981	2.70	2.59
1986	2.42	2.48
1990	2.37	2.53
1996	2.35	2.52
1997	2.38	2.53
1998	2.43	2.55
1999	2.47	2.54
2000	2.30	2.64
2001	2.45	2.64
2002	2.49	2.62
2003	2.47	2.48

Source: Calgary Civic Census

According to the 2003 census, 27% of the dwellings contain one person, 40% two people, 18% three people and 16% four or more people.

It is interesting to note that in the year 2003, occupancy levels were highest in townhousing (2.9 persons/dwelling) and duplex/semi-detached (2.6 persons/dwelling) housing forms as compared to single family dwellings (2.3 persons/dwelling).

## **5.5 Population by Dwelling Type**

In the year 2003 the majority of people who live in Montgomery live in single family dwellings (65%) followed by duplex/semi-detached (18%), converted dwellings (5%), nursing facilities (6%), townhousing (4%) and apartments (1%).

## **5.6 Home Ownership**

The 2003 civic census indicates that Montgomery has a slightly lower rate of home ownership (62%) as compared to Calgary (70%). Home ownership of single family dwellings (76%) is lower as compared to the city (91%).

**Table 4: Home Ownership by Structure Type (2003)**

Type	Montgomery	Calgary
Single Family	76%	91%
Duplex	56%	60%
Apartment	0%	27%
Townhousing	0%	57%
All Types	62%	70%

Source: Calgary Civic Census 2003

## **5.7 Income Indicators**

Average family incomes are significantly lower in Montgomery as compared to Calgary. In 2001, the average family income in Montgomery was almost \$55,710 whereas the city average was almost \$73,663.

When family incomes are evaluated, Montgomery has significantly more low-income families (25%) as compared to a city average of 15%. Montgomery also has a greater than average proportion of renters paying in excess of 30% of their income on rent, 45% compared to a city average of 36%.

## **5.8 Language & Immigration**

In 2001, almost 2% of Calgary's population did not have strong English or French speaking skills compared to 4% in Montgomery. In 1996 22% of Calgarians were born in another country while 14% of the residents in Montgomery were immigrants.

## **5.9 Aboriginal Population**

Montgomery has a larger than average Aboriginal population, 6% of the population as compared to 3.5% for the city as a whole.

## **5.10 Household Types**

Montgomery has a greater than average proportion of lone-parent families, 22% compared to a city average of 15%. It also has a greater proportion of the population living alone, 13% compared to a city average of 9%.



## **6.0 RESIDENTIAL BACKGROUND INFORMATION**

### **6.1 Issues**

Based upon the mail-out resident survey completed in November 2000, a number of common residential themes emerged including:

- Montgomery has a unique small town character and this should be preserved;
- higher density residential developments such as apartments and fourplex buildings could destabilize the community;
- some people felt that more people should live in Montgomery and housing options such as higher density should be considered;
- concern with the fit of infill development and cookie cutter housing development;
- views and topography were identified as important characteristics of the residential environment;
- concern with run-down properties.

Relating to residential development, the volunteer Community Planning Advisory Committee identified Montgomery's topography, views, location, and small town character as community strengths. Other strengths identified included mature vegetation, large lots, and access to the downtown and to the west city limits. The Committee identified Montgomery's poor city-wide community image as a weakness.

### **6.2 Current Residential Land Use**

Montgomery is predominantly a low density residential community 47% of Montgomery's gross developable land area is designated (zoned) for low-density residential development. Of this area, 23% is designated for single detached dwellings (R-1) and 24% is designated for duplex/semi-detached dwellings (R-2).

A very small percentage (1%) of the gross developable area is designated for low density townhouse style development (R-2A). The only land designated for higher density residential development consists of 5 ha (12 acres) on the Direct Control site (DC 62Z92) located by the Bow River and 13 Avenue. In 1992, this site was

designated by Council for a comprehensive development consisting of apartments and townhouses to a maximum of 234 units. Subsequent to the land use designation, The City of Calgary purchased this site as well as the majority of private lands between the Bow River and 13 Avenue. (See the Existing Land Use Designations Map). *The Bowmont Design Brief* (1978) recommended that the area between the Bow River and 13 Avenue be acquired on an opportunity basis by The City of Calgary to facilitate the integration of these lands into Shouldice Park and the river park system. *The Urban Park Master Plan* (1994) supports this policy direction.

**Table 5: Residential Land Use – Montgomery (2000)**

Land use Designation (Zoning)	# of Parcels	Hectares	Acres	% of Developable Gross Area	% of Total Residential Area
R-1/DC R-1/Vacant R-2	552	38	94	24%	45%
R-2/DC R-2/Vacant R-2	788	39	96	24%	46%
R-2A	27	2	5	0%	3%
DC Multi-Family	4	5	12	3%	6%
<b>Total</b>	<b>1,371</b>	<b>84%</b>	<b>207</b>	<b>48%</b>	<b>100%</b>

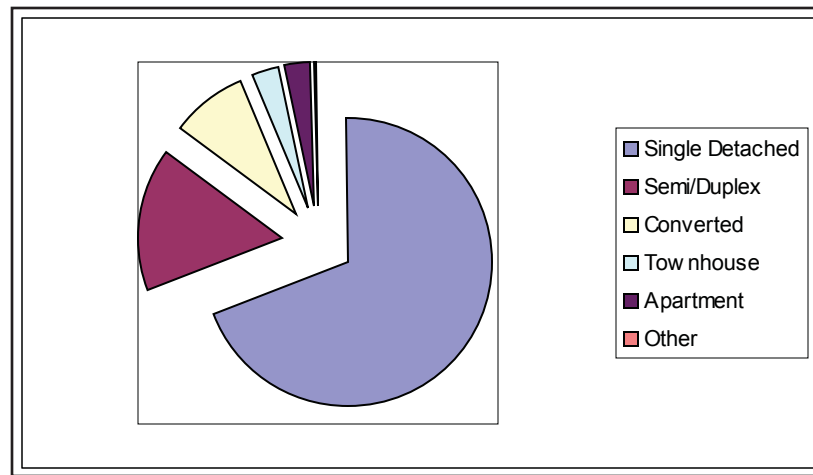
Source: Land Use Planning

**Table 6: Number of Dwelling Units by Structure Type (2003)**

Type	Number of Units	% of Total
Single Family	1,136	68%
Duplex	271	16%
Apartment	149	9%
Townhousing	51	3%
All Types	43	3%
Other	14	<1%
<b>Total</b>	<b>1,664</b>	<b>100%</b>

Source: Calgary Civic Census

Almost 70% of dwelling units in Montgomery are single detached dwellings, followed distantly (16%) by duplex and semi-detached dwellings.



### 6.3 Residential Density

A corner stone of the City's strategy for growth management involves addressing land use, transportation and community density. Calgary's existing communities are encouraged to house more people in a manner that is sensitive and compatible with the community.

*The Calgary Plan* advocates two major approaches to residential development, first to increase the range of housing types and second to increase housing densities. According to *The Calgary Plan* the benefits of these approaches include:

- Minimization of community population loss as they mature through the community lifecycle.<sup>1</sup>
- Provision of housing choice, including affordable housing.
- Provision of a sufficient population base to support local goods and services within walking distance of many households.
- Reduction of the per capita costs of urban development.
- Reduction of the land required for future development.
- Increasing transit ridership and a more cost effective transit service.

The City of Calgary's policy direction for new communities is that the residential density should achieve a target density of 6-8 units per gross residential acre (*The Calgary Plan & Sustainable Suburbs Study*). The City does not set a residential density target for Calgary's established communities. Montgomery currently has a residential density of 4.3 dwelling units per gross acre. The following table provides a density comparison with other Calgary communities.

**Table 7: Residential Density Comparison**

Community	Units Per Gross Residential Acre
Montgomery	4.3
Silversprings	4.4
Hawkwood	4.5
Parkdale	4.8
Ogden	5.0
West Hillhurst	6.1
Dover	6.3
Hillhurst	9.5
Sunnyside	13.7

## 6.4 Historic Development Activity

Development activity as illustrated through the issuance of development permits for single detached and duplex/semi-detached dwellings has been moderate and stable through the 1990s. On average, six to seven development permits for single detached dwellings were issued per year between 1990 and 2000. During the same time period, an average of three development permits per year were issued for duplex/semi-detached dwellings. Approximately four development permits were issued per year for additions, renovations and garages during the 1990s. No multi-unit development permits were issued during this period.

## 6.5 Development Potential

Considering Montgomery's existing land use designations, a significant degree of residential redevelopment potential exists in the community's R-2 areas. It was determined that the population could increase by approximately 24% if all the practically developable R-2 lands were developed to their maximum potential. It is important to stress that this population increase is considered extremely unlikely and theoretical. Most inner communities in Calgary have not developed to their maximum capacity and if Montgomery's population in existing R-2 areas were to increase by 24%, its population would still fall short of the community's peak population reached in 1968.

Although Montgomery has excellent access to downtown and a quality living environment, residential redevelopment activity has not, to date, been as active as other inner city communities. Given regional and city wide growth and Montgomery's existing land use, there is potential for housing development and redevelopment to occur Montgomery.

## 6.6 Montgomery's Excluded Status

Montgomery's status as 'excluded' from *The Land Use Bylaw 2P80* rules for Modest Residential Development was reviewed as background to the community planning process.

*The Land Use Bylaw 2P80* classified Montgomery as an Established Community but Excluded from the rules for Modest Residential Development. **Being excluded from the modest rules means that:**

- All new dwellings (single, semi detached & duplex) are considered as Discretionary Uses (infill guidelines apply) and therefore a Development Permit is required. All new dwelling applications are circulated to the community association for comment, the site is posted, and the decision can be appealed by the community or applicant to the SDAB (Subdivision and Development Appeal Board).
- Section 8 (2) (d.4) (five foot height rule) of *The Land Use Bylaw 2P80* applies only to Excluded Communities and allows external additions, without a Development Permit, to single, semi and duplex dwellings provided that:
  - the addition does not exceed five feet in height above the roof ridge;
  - the addition meets the minimum requirements of *The Land Use Bylaw 2P80*.

For contrast, the following rules apply to communities included in the Modest Rules:

- All new dwellings and additions that meet the rules for Modest Residential Development (LUB Section 20.1) are considered to be Permitted Uses and a Development Permit is not required (only a Building Permit). Dwellings will be approved if they comply with the Modest Residential rules. No relaxations to *The Land Use Bylaw 2P80* are permitted and the Infill Guidelines are not used to evaluate the application. The Building Permit application will not be circulated to the Community Association and there are no opportunities for community appeal.
- Section 8(2)(d.4) (five foot height rule) does not apply.

### **Permit Trends**

- Since Montgomery became an excluded community in 1995, the majority of dwelling additions have proceeded straight to Building Permit. To date, 63% (24 out of 38) of dwellings additions were issued under a Building Permit and 37% of dwelling additions required a Development Permit (14 out of 38).

### **Advantages & Disadvantages of Excluded Status**

The following table provides an overview of advantages and disadvantages associated with Montgomery's status as an 'Excluded Community' from the rules for Modest Residential Development.

In summary, Montgomery's excluded status ensures that the Community Association Planning Committee has the opportunity to review all new dwelling applications. These applications will be evaluated with the assistance of community input and against the requirements of *The Land Use Bylaw 2P80* and the Infill Guidelines. The major disadvantages are that landowners are not encouraged to construct smaller more modest homes and significant additions can be made (without Development Permit) to both single detached and semi-detached in conformance with the LUB 'five foot height rule'.

Montgomery's excluded status ensures that all new dwellings require a Development Permit and community review. The excluded status also means that significant additions to existing dwellings can be constructed without a Development Permit.

Advantages of Montgomery's Excluded Status	Disadvantages of Montgomery's Excluded Status
<ul style="list-style-type: none"> <li>• All new dwelling applications are circulated to the Community Association for review and input. This encourages community context to be factored into the decision making process. The CA planning committee will always know about applications for new dwellings (no surprises).</li> </ul>	<ul style="list-style-type: none"> <li>• For Montgomery, the approval process for a small scaled dwelling is the same as a large scaled dwelling therefore property owners are not provided an incentive to build modest sized dwellings. Modest sized dwellings would probably fit well within the community context.</li> </ul>
<ul style="list-style-type: none"> <li>• All new dwellings would be considered as Discretionary Uses, therefore the community has the opportunity to appeal an approval to the Subdivision and Development Appeal Board.</li> </ul>	<ul style="list-style-type: none"> <li>• Section 8(2)(d.4) (five foot height rule) of <i>The Land Use Bylaw 2P80</i> applies and allows significant additions without a Development Permit. Building roof height can be raised by 5 feet and the building can expand to the minimum requirements of the Land Use Bylaw. A dwelling can legally triple in size utilizing the "five foot height rule". Alternatively, the Modest Rules provide specific regulations that ensure additions (approved with a Building Permit) are low impact. Montgomery does not have this benefit.</li> </ul>
<ul style="list-style-type: none"> <li>• Infill Guidelines apply to all dwelling applications. This allows aesthetics &amp; community context to be considered in the City's decision making. Alternatively, the Infill Guidelines do not apply to Modest applications in the included communities.</li> </ul>	<ul style="list-style-type: none"> <li>• Property owners with small dwelling proposals do not have a quick route through the Development Approvals process.</li> </ul>
<ul style="list-style-type: none"> <li>• All semi-detached dwellings require Development Permit. Some poor design has been reported for semi-detached dwellings approved under the Modest Rules.</li> </ul>	



## **7.0 COMMERCIAL BACKGROUND INFORMATION**

### **7.1 Overview**

As part of the community planning process, The City of Calgary conducted a mail-out survey of Montgomery residents in the fall of 2000. Amongst other issues, the results provide a general overview of resident issues relating to commercial development in the Montgomery. Resident concerns included:

- Concerns that the Bowness Road commercial area is an eyesore and should be improved. Recommended improvements included the streetscape environment, reorganized parking, improved facades and new uses.
- A number of people recognized that the Bowness Road commercial area helps create a small town character and could be an asset if revitalized.
- While the majority of comments on the topic of commercial development were related to Bowness Road some people mentioned the need to upgrade the Trans Canada Highway in order to both enhance the community and provide a better entrance to the city.
- A few people mentioned concerns relating to the emissions and industrial intensity of the Rosedale Laundry operation and with the 'Raves' at the paint-ball operation on MacKay Road.

Analysis completed by the Montgomery Plan Committee (Community Planning Advisory Committee) parallels the comments of the general public. This committee identified one of Montgomery's top strengths as its small town character and its potential to improve. Commercial development on Bowness Road was identified as communicating a negative community image yet offers potential to establish a new community image.

The community of Montgomery contains three commercial areas including:

- (a) Bowness Road & 46 Street General Commercial
- (b) Trans Canada Highway Commercial
- (c) Safeway & Montgomery Shopping Centre

## 7.2

### **Bowness Road & 46 Street Commercial Area**

This area includes lands designated for commercial development on the north and south sides of Bowness Road between 45 and 47 Streets and includes 46 Street between Bowness Road and 16 Avenue. The area is currently designated with a mix of General Commercial (C-3) and Direct Control (C-1 guidelines).

The CPAC committee recognized the need for economic and environmental revitalization on Bowness Road and that this requires both commercial and residential uses. Higher density residential development in the strategic locations proposed in the Plan would increase the local customer base for commercial services on Bowness Road.

The CPAC identified a preferred future for Bowness Road, that contained the following elements:

- Improved building and streetscape appearance;
- Focus resources on street trees and vegetation;
- Slower moving traffic achieved through traffic calming measures;
- Safe and pedestrian oriented;
- Mix of commercial and residential land uses to a maximum of three storeys;
- Bowness Road commercial evolves into a destination place;
- Increased residential density in specific locations along Bowness Road;
- Change name of Bowness Road to reflect Montgomery's image and uniqueness;
- Easier access to Bowness Road for residents to the north of Bowness Road to head east;
- Surface parking to encourage use of the area.

## Property Line Setbacks

All new development along Bowness Road must comply with *The City of Calgary Land Use Bylaw 2P80* which requires a 5.18 metre (17 feet) property line setback along both the north and south sides of Bowness Road from 51 Street to MacKay Road. This road-widening setback is considered as an obstacle to redevelopment and the eventual creation of a more 'pedestrian friendly' and transit supportive commercial area. In urban design terms it is preferable that new development be pushed as close to the front property line as possible. This supports a more intimate and pedestrian friendly environment and reinforces slower traffic movements. The existing setback would push the development back from the front sidewalk losing the connection with the street and pedestrian movement.

### 7.3 16 Avenue – Trans Canada Highway Commercial Area

The Trans Canada Highway strip is currently designated as C-6 Highway Commercial. The purpose of district is to provide for commercial uses that serve the motoring public and are located on streets with heavy traffic volumes. The actual land uses on 16 Avenue through the community of Montgomery include a large number of auto related uses as well as uses oriented to the travelling public. Approximately 3.2 ha (8 acres) of land are occupied by highway oriented uses and .26 ha (.6 acres) are vacant.

In the mail-out survey residents expressed a concern regarding pedestrian safety and the need for enhanced pedestrian linkages between north and south Montgomery. Residents also expressed a concern with the visual appearance of 16 Avenue. A number of policy recommendations currently exist for improving the Trans Canada Highway as stated in the Council approved report entitled *Improving Calgary's Entranceways* (adopted in 1994).

The report recommends the following regulations for the section of Trans Canada Highway through Montgomery:

- billboards should be placed 250 metres apart;
- portable signs should be prohibited;
- except for real estate signs, which must be attached to buildings below the roofline, temporary signs should be prohibited;

- buildings adjacent to the front property line should have parking provided behind the principle building. In the event that parking is adjacent to the roadway, it should be screened by an opaque fence, not less than one metre high, and cluster landscaping should be incorporated to soften the appearance of the solid structures;
- Recreational or automobile dealerships should setback their vehicle inventories 2.1 metres (7 feet) from the roadway and be screened by a fence not less than one metre in height.

### **Property Line Setbacks**

The Trans Canada Highway, 16 Avenue, is a major component in Calgary's road network and is classified as both a major roadway and expressway. *The City of Calgary Land Use Bylaw 2P80* requires that future development provide a 15.2 metre (50 ft.) setback, 10.363 m is required on the south side only. The setback for development along the Trans Canada is required to support citywide transportation requirements.

### **Pedestrian Connections and Safety**

While not a direct land use issue, concerns regarding pedestrian safety on 16 Avenue were strongly expressed by the public in the fall 2000 mail-out survey.

### **Aesthetics**

Area residents identified the appearance of the Trans Canada Highway as a concern.

## **7.4 Safeway & Montgomery Shopping Centre**

*The Bowmont Design Brief (1978)* recommended that the Montgomery Shopping Centre be encouraged to develop a more cohesive circulation system. With the redevelopment of the Safeway site in 1997-98 a number of circulation problems appear to have been resolved. The area is currently designated as a combination of C-3 General Commercial and Direct Control for the Safeway site. While a few people expressed concern with the safety of access to the Safeway site from 16 Avenue, the issue of circulation around the Shopping Centre was not a top concern. A number of people supported the convenience of the commercial uses located in this area and the commercial uses located to the west of Dairy Queen.

### **C-3 Land Use Designation on East Side of Home Road between 16 and 17 Avenues**

On Home Road, there are four parcels currently used for single detached dwellings that are designated as C-3 General Commercial. The Plan proposes to restrict the height to 16 m of any future development in this location.

### **MacKay Road**

A low impact commercial use is located on MacKay Road and contains mini-storage and paint ball uses. This site was designated Direct Control in 1989 and transitioned from an under-used private club to a mini-storage facility. The zoning allows the permitted and discretionary uses of the A-Agricultural and Open Space District with the additional discretionary use of inside storage. A range of other discretionary uses (gaming establishments, bingo, horse riding academies, cemeteries, crematoriums, intensive agriculture and kennels) were removed at the same time. A number of residents mentioned concerns with the Rave dances that have been held in the facility. The City of Calgary recently adopted a new Rave Bylaw and this could help mitigate resident concerns.



## **8.0 TRANSPORTATION BACKGROUND INFORMATION**

### **8.1 Road Constraints**

Constraints in the road network contribute to some local traffic concerns, most of which relate to traffic short cutting through the community. A study carried out as part of the traffic planning for the proposed Market Mall expansion summarized the primary reasons for the short cutting traffic in Montgomery as due to:

- The existing roadway network configuration.
- Expanding urbanization including the Trans Canada Highway growth corridor as well as the residential communities along Crowchild Trail.
- Increasing traffic congestion in all areas of Calgary. This is exacerbated by the concentration of major residential areas in the west side of the city and the employment areas on the east side.
- The proximity of major traffic generators such as Market Mall and the University of Calgary.

In particular, the alignment of the Trans Canada Highway through Montgomery and the connections between this roadway and the Shaganappi Trail has resulted in a shorter distant route via Home Road and 32 Avenue NW. This situation is compounded by the lack of connections between Shaganappi Trail and the Trans Canada Highway for traffic that is orientated to or from the north and the west.

As a result, the traffic oriented to 32 Avenue and other communities to the north use 32 Avenue and Home Road to gain access to the Trans Canada Highway. Similarly, traffic orientated to the community of Bowness, traffic along 32 Avenue and further to the north also use Home Road to make the connection. 48 Street and 52 Street are also used by traffic making this shortcut pattern. Northbound shortcut traffic also uses 48 Street when there are left turn delays at the Home Road and Bowness Road signalized intersection.

## **8.2 Future Development**

Unless preventative measures are put in place short cutting traffic volumes are likely to increase in the future due to the following developments

- The expansion of Market Mall from approximately 750,000 square feet to 900,000 square feet by 2005, which is the equivalent to approximately 300 new jobs.
- The Foothills Hospital expansion, which includes approximately 750 new jobs by 2005 and another 550 new jobs by 2020.
- The proposed relocation and expansion of the Alberta Children's Hospital into the University's, West Campus lands, which includes 1,700 new jobs by 2005.
- The proposed growth in the University of Calgary Main Campus, as well as the anticipated development of the University's West Campus lands. This includes 750 new jobs at the main campus by 2005 and another 2,000 new jobs by 2020.
- The development of the Motor Vehicles site in Parkdale, housing 960 people in a multi-dwelling site, 350 bed units for seniors and extended care and 1,000 jobs by 2020.
- New residential development in the northwest.

## **8.3 The Montgomery Community Traffic Study**

A Resident Traffic Committee was established to work with The City of Calgary to carry out a Community Traffic Study, the results of which have been integrated into this Plan.

### **Identification of Issues**

A preliminary survey was mailed in the fall of 1999 to all Montgomery residents to identify key issues and concerns. These survey findings were supplemented by the resident feedback at three open houses held on traffic (March 2000), the Market Mall expansion proposal (September 2000), and this Plan (November 2000).

In total the community consultation exercise resulted in approximately 700 written responses. The most frequently mentioned traffic problems were related to excessive speed, high traffic volumes, high truck volumes, short cutting traffic, cyclist vehicle conflicts, parking issues and pedestrian safety. The most frequently mentioned locations to which these problems related included Bowness Road, Home Road, 32 Avenue, 48 and 52 Streets. The results also indicated that problems occurred at these locations everyday and were not restricted to rush hour times.

### **Development of a Traffic Plan**

The next stage was to develop a Traffic Plan too reduce or eliminate shortcutting traffic. The Resident Traffic Committee presented the main traffic control options to the community in an opinion survey and at a Community Open House.

### **Implementation**

Based upon the results of the Traffic Survey, the Open House and the Traffic Committee discussion, trial implementation of improvements for Home Road were agreed upon. The lanes on Home Road have been narrowed and an on-street bike lane installed on a trial basis. The addition of the northbound bike lane provides a safer route for cyclists on one of the busiest cycle routes in the city. Temporary curb bulbs have also been installed at 19 and 20 Avenues.

The remaining proposed traffic calming alternatives examined by the Traffic Committee did not generate enough community support to warrant trial implementation. The Traffic Study also provided a useful forum to discuss the impacts and mitigation measures of the proposed Market Mall expansion. A proposal to improve the Shaganappi Trail interchange to make access to and from the Trans Canada Highway easier and thereby encourage traffic to take this route rather than short cutting through Montgomery was proposed during this process.

### **Pathway and Bikeway Constraints**

A number of constraints in the bikeway and pathway system were identified through the Montgomery Traffic Study and *The Calgary Pathway and Bikeway Plan* as detailed below.

- The one-way on street bikeway on 52 Street causes as significant break in the pathway network for cyclists travelling northwest.
- A pedestrian overpass is required over 16 Avenue (the Trans Canada Highway) to link the community and provide safer connections to the River Valley.

- A pedestrian overpass is required over the Shaganappi Trail to link Montgomery to the University Endowment Lands (future Alberta Children’s Hospital site).
- Curb cut upgrades required at the Bow River pathway and on Home Road.
- A bikeway connection is required from MacKay Road to the Market Mall access traffic signal. This connection upgrade could be made when the Market Mall access is upgraded.

## 8.4 Bowness Road Streetscape Improvement Concept

A streetscape improvement strategy for the enhancement of the pedestrian environment of Bowness Road, from Home Road to MacKay Road, with special emphasis on the commercial area between 47 and 49 Streets NW was prepared in conjunction with the Area Redevelopment Plan to provide a framework for redevelopment. This area was identified as requiring streetscape enhancement in order to support commercial revitalization, pedestrian safety, and improve the overall aesthetics/image of the community. The CPAC committee determined during a visioning process that a “Clean and Green” concept emphasizing vegetation and pedestrian comfort should drive the streetscape improvement design philosophy. Traffic calming and entry enhancements supported by the Montgomery Community Traffic Study, and identified as policy direction, have informed the development of the streetscape improvement strategy.

### Streetscape Design Goals

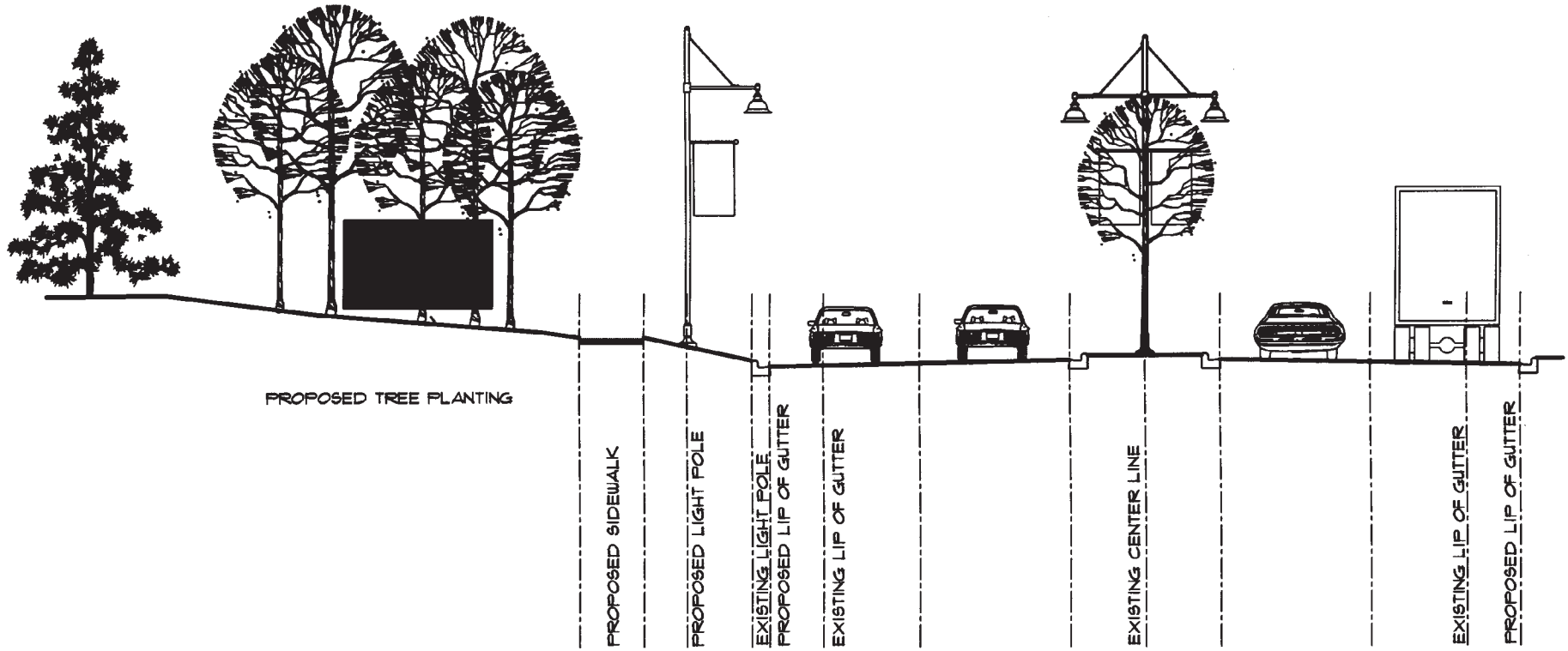
The goals of the streetscape design development plan set out to:

- (a) Build upon and complement the work that has already gone into studies of this area:
  - Focus on creating a pedestrian friendly neighbourhood.
  - Employ traffic calming measures.
  - Enhance sidewalks and crosswalks.
  - Enhance the commercial area with a higher level of detailing.
  - Accommodate potential future commercial and residential development.

(b) Create a distinctive look for the Montgomery Streetscape that reflected the community's "Clean and Green" vision for itself.

- Create an attractive and welcoming streetscape for the Montgomery Community.
- Create an unified and harmonious streetscape image.
- Define street edge with tree planting and a coordinated set of streetlights and site furnishings.
- Use contemporary street furnishings to avoid historical references.
- Employ the extensive use of vegetation.
- Rationalize existing parking at commercial area.

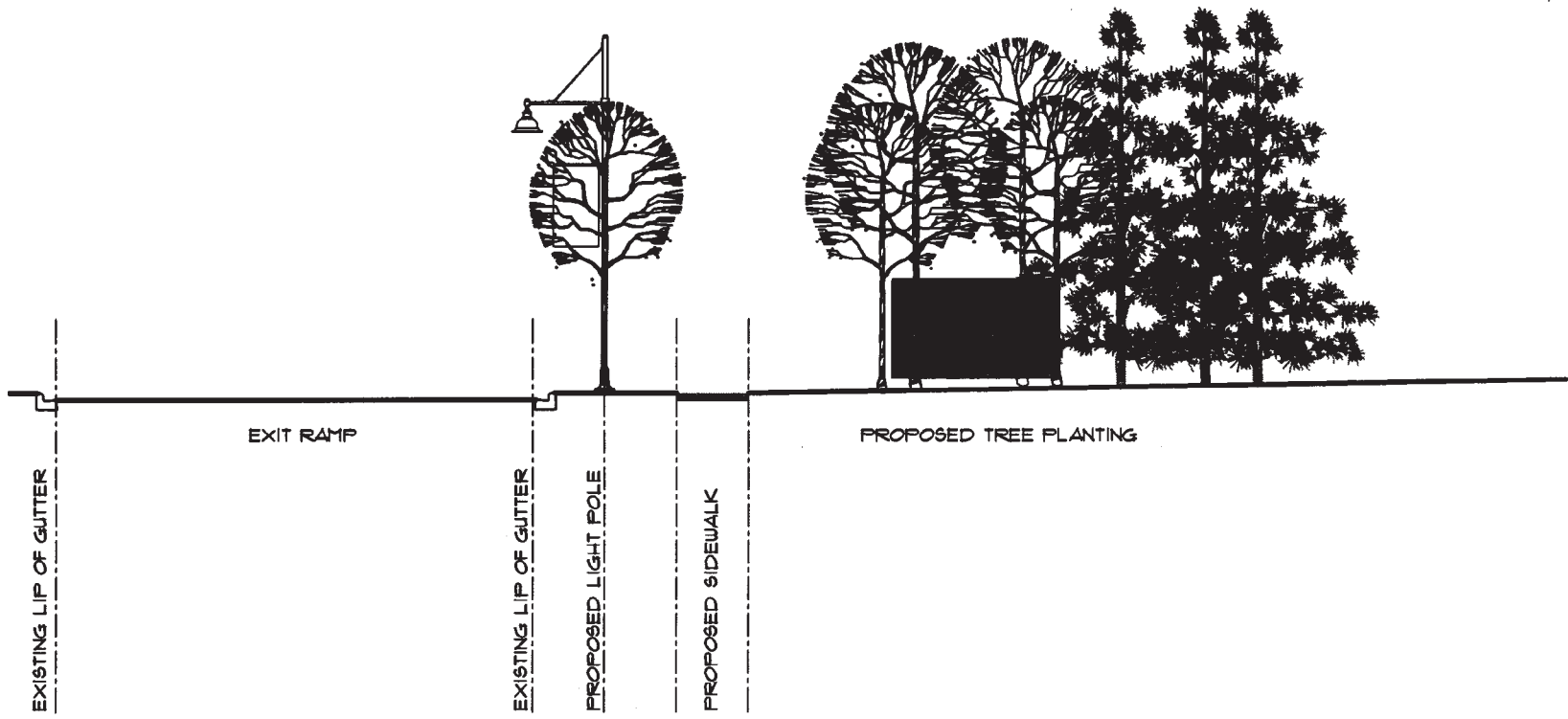
The following plans, sections and sketches illustrate how the various elements of the proposed streetscape; such as walkways, crosswalks, paving patterns, tree planting, lighting, driving lanes, parking lanes and street furnishings fit together to create a visual identity for the Montgomery streetscape. The sketches also promote a harmonious streetscape through the potential future building massing, entrances, window heights, rooflines and building setbacks.



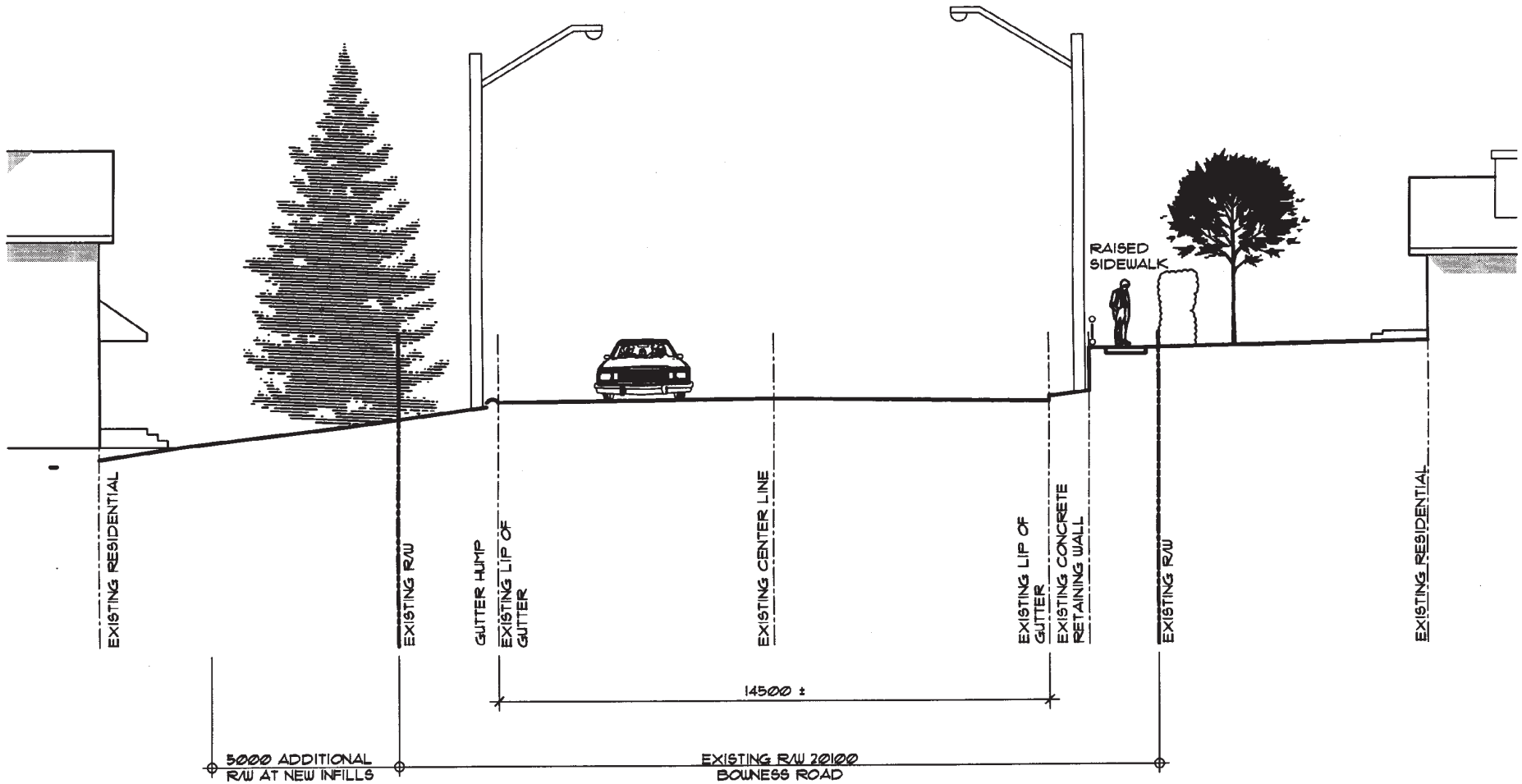
## EAST ENTRY STREETSCAPE IMPROVEMENTS

SECTION SCALE 1:150

*Note: This cross section is for illustration only and the details and dimensions indicated have not been approved.*



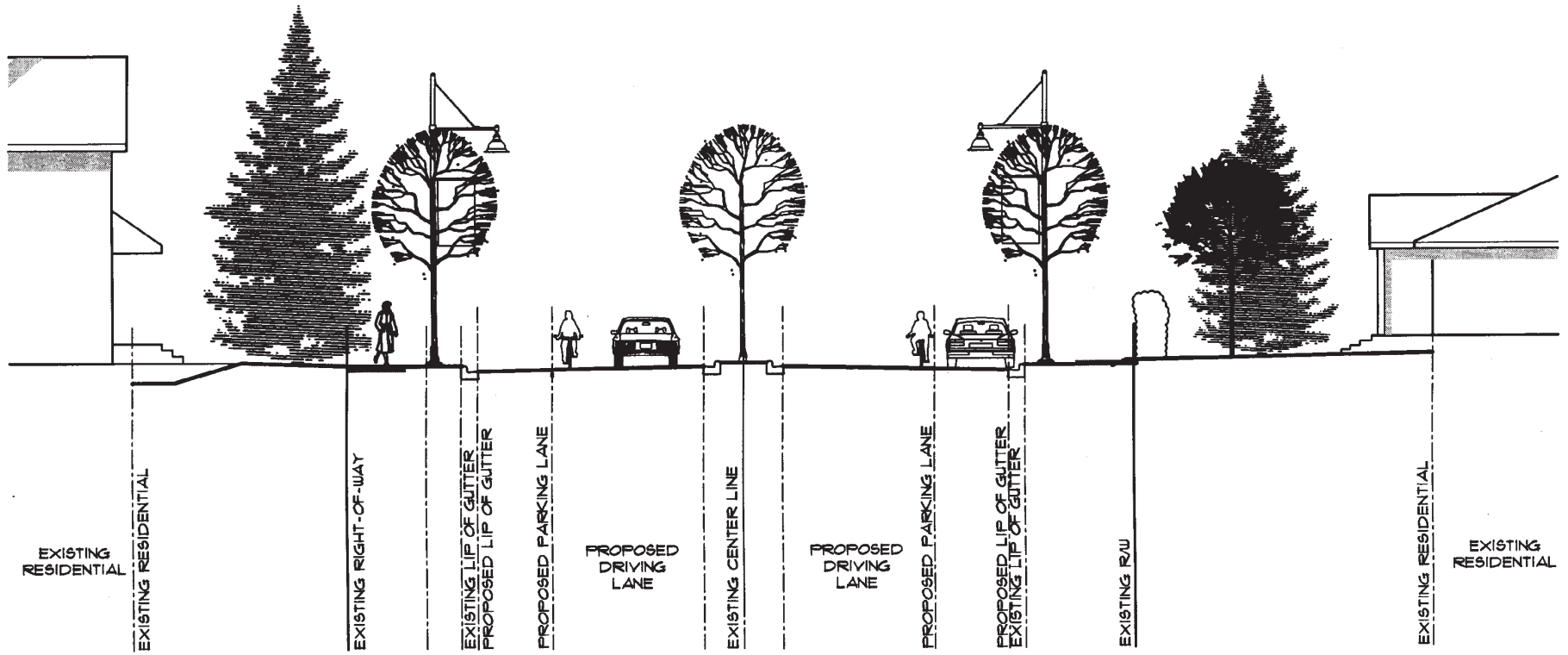
*Note: This cross section is for illustration only and the details and dimensions indicated have not been approved.*



## EXISTING CONDITIONS: 4500 BLOCK RETAINING WALL

SECTION SCALE 1:150

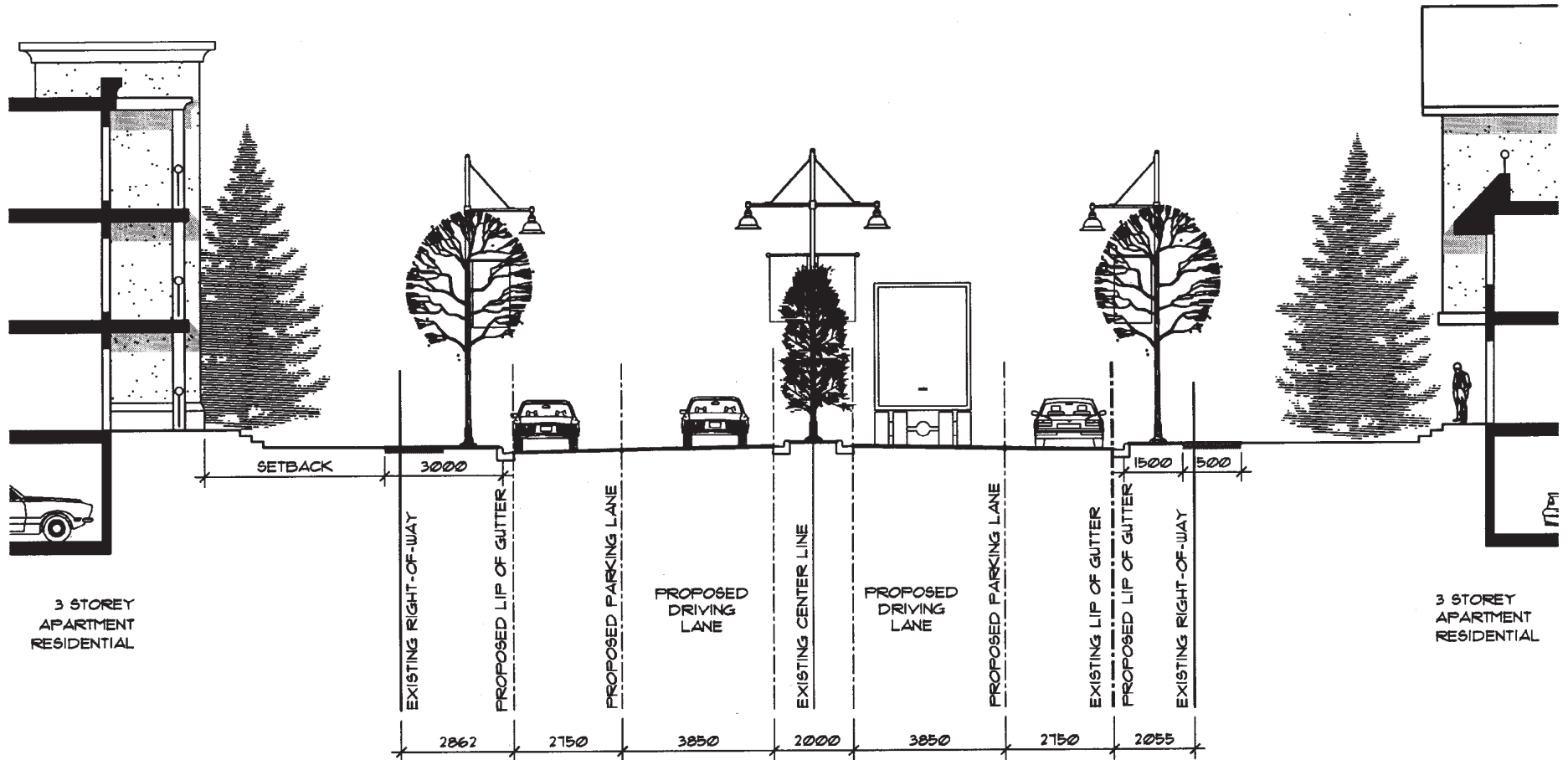
*Note: This cross section is for illustration only and the details and dimensions indicated have not been approved.*



## TYPICAL RESIDENTIAL: PROPOSED STREETSCAPE WITH EXISTING RESIDENTIAL

SECTION SCALE 1:150

*Note: This cross section is for illustration only and the details and dimensions indicated have not been approved.*



## TYPICAL RESIDENTIAL: PROPOSED STREETSCAPE WITH POTENTIAL RESIDENTIAL DEVELOPMENT

### SECTION SCALE 1:150

*Note: This cross section is for illustration only and the details and dimensions indicated have not been approved.*

# Glossary

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<b>Accessory Building</b>	A building that is secondary or minor to the principal use of a site and is detached above grade from a principal building.
<b>Accessory Use</b>	A use that is subordinate or incidental to the principal use of the site.
<b>Adjacent Land Owner</b>	The property owner (listed on the City tax roll) of the land next to the site on which an application is being processed.
<b>Alternate Parking</b>	Off-site parking stalls (usually within 400 feet) that are used to meet the parking requirements for the proposed development.
<b>Approving Authority</b>	The Calgary Planning Commission, the Development Officer, the Subdivision Authority and/or the Subdivision & Development Appeal Board.
<b>Area Redevelopment Plan (ARP)</b>	A statutory plan that identifies planning goals and objectives of residents, owners and business people in an existing area. It is a basic community planning document that deals with zoning, traffic, parks, social issues, etc. and is prepared in close consultation with residents, owners and business people.
<b>Automotive Service</b>	A business for the sale of gasoline and oil with other possible uses including a one-vehicle car wash, vehicle repair and a grocery store.
<b>Automotive Speciality</b>	A business for servicing or repairing vehicles, such as a muffler shop, transmission repair and car washes.
<b>Basic Right-of-Way</b>	<i>The Land Use Bylaw 2P80</i> , Section 17 table sets out the Basic Right-of-Way required for public thoroughfare purposes for the roads within Calgary.
<b>Building Permit</b>	A building permit issued by The City to erect a new building or structure or to demolish, relocate, repair, alter or make additions to an existing building or structure.
<b>Built Form</b>	The layout (structure and setting on a site), density (height, storeys, and mass) and appearance (materials and details) of a development.

<b>Business Revitalization Zone (BRZ)</b>	A group of business people who administer funds, collected through a special business tax, to improve the area and jointly promote their businesses.
<b>Certainty of Use (C.U.)</b>	A category of discretionary uses in the Land Use Bylaw that cannot be refused on the basis of use – for example, an apartment in RM-4. Projects may, however, be refused for other reasons such as inadequate parking.
<b>Change of Use</b>	A type of Development Permit required when the use is changing from a permitted use to a discretionary use (e.g., restaurant to drinking establishment in C-2) or one discretionary use to another discretionary use (e.g., child care facility to private school in C-2).
<b>Community Character</b>	Montgomery has a range of characteristics that contribute to its small town character and are reflective of its historical development as a separate municipality (Montgomery was annexed to Calgary in 1963). These characteristics include a traditional grid street pattern, predominantly low density environment and well defined community boundaries. Other features that contribute to its small town character include its local ‘main street’ commercial area concentrated on Bowness Road, a large stock of mature trees spread throughout the community and a high proportion of public open space. The Background Section provides details of the community’s historical development and context.
<b>Condominium</b>	A building containing units that are individually owned, could be a warehouse, an apartment, townhouse, etc.
<b>DC (Direct Control)</b>	The purpose of this district is to provide for developments, that, due to their unique characteristics, innovative ideas, or because of unusual site constraints, require specific regulations unavailable in other land use districts. This district is not intended to be used in substitution of any other land use district in the Land Use Bylaw that could be used to achieve the same result.
<b>Density</b>	The number of dwelling units per acre or hectares (restricted). The ratio of floor space or building area to the site area (commercial).
<b>Development Design Guidelines</b>	Design suggestions that supplement the rules of the Land Use Bylaw. The most often used is “Low Density Residential Housing Guidelines for Established Communities.”
<b>Development Permit (DP)</b>	A document authorizing a development, issued by the Approvinig Authority that includes plans and conditions of approval and establishes form, intensity and appearance. A Building Permit may also be required.
<b>Discretion</b>	The term used when the Approving Authority varies any of the rules of the Land Use Bylaw.

<b>Discretionary Use</b>	<p>All discretionary uses require a development permit. Unlike permitted uses, a discretionary use may be refused if the use does not fit in the proposed location or if the Development Authority believes it would adversely impact the area.</p> <p>Certain discretionary uses, however, that are marked C.U. (Certainty of Use) in the Land Use Bylaw cannot be refused on the basis of use only.</p> <p>The Development Authority must evaluate the application, on its merits, having regard to Council approved plans and policies, the rules (minimum standards) of the Land Use Bylaw and the local context.</p>
<b>Downzoning</b>	A change of land use designation that decreases the allowed density or intensity of use, for example C-3 to C-2 (16).
<b>Duplex</b>	A single building containing two dwelling units, one above the other, each having a separate entrance.
<b>Dwelling Unit</b>	Two or more rooms that have kitchen, living, sleeping and sanitary facilities.
<b>Eaveline</b>	The line formed by the intersection of the wall and the roof of the building.
<b>Elevation Plan</b>	A drawing of the front, side or rear of a building.
<b>Established Communities</b>	Older communities that are subject to special rules for some types of residential development. A map and list of the communities are in the Land Use Bylaw.
<b>Façade</b>	The front of a building, or any face of a building that has been given special treatment or attention.
<b>Floodplain</b>	Those lands abutting the floodway, the boundaries of which are indicated by floodwaters of a magnitude likely to occur once in one hundred years.
<b>Floodway</b>	The river channel and adjoining lands indicated on the Floodway/Floodplain Maps, that would provide the pathway for flood waters in the event of a flood of a magnitude likely to occur once in one hundred years. This land is dedicated as Environmental Reserve when subdivision over 0.8 hectares (2 acres) occurs.
<b>Floor Area Ratio (FAR)</b>	The ratio of the gross floor area of a building to the gross site area.
<b>Garage</b>	An accessory building or part of a principal building designed and used for the shelter or storage of vehicles including a carport.

<b>General Municipal Plan (GMP)</b>	See <i>Municipal Development Plan</i> .
<b>Grade</b>	The elevation of the finished ground surface (excluding an artificial embankment) at any point immediately adjacent to the building, referenced to the geodetic datum.
<b>Gross Floor Area</b>	A total of all the floor area above grade measured from the outside walls.
<b>High Density Residential</b>	Residential development above four storeys includes RM-6 and RM-7.
<b>Housing Type</b>	Categories of dwelling units (regardless of ownership). The categories are: single-family (single-detached dwellings); two-family (duplex, semi-detached and additional dwelling units); and multi-family (triplex, fourplex, townhouse, and apartment buildings).
<b>Infill</b>	Development that occurs on a vacant site after completion of the initial development of the area.
<b>Institutional Uses</b>	Public or private uses that serves the educational, social, cultural, or religious needs of the residents in a community and may include a church, a post office or postal kiosk, a library, a public or private school, and a child-care facility.
<b>Landscaping</b>	<p>The modification and enhancement of a site by:</p> <ul style="list-style-type: none"> <li>a) soft landscaping consisting of vegetation such as trees, shrubs, hedges, grass and ground cover;</li> <li>b) hard landscaping consisting of non-vegetation material such as brick, stone, concrete, tile and wood, excluding monolithic concrete and asphalt; and/or</li> <li>c) architectural elements consisting of wing walls, sculptures and the like.</li> </ul>
<b>Land Use Amendment</b>	A change of land use designation, approved at a public hearing of City Council.
<b>Land Use Bylaw 2P80</b>	The City of Calgary Land Use Bylaw that establishes procedures to process and decide upon land use and development applications and divides the city into land use districts. It sets out rules that affect how each piece of land in the city may be used and developed. It also includes the actual zoning maps.

<b>Land Use Designation (Zoning)</b>	The legal control on the use and intensity of development on a parcel of land (not on the design of a project).
<b>Land Use District</b>	An area of the city designated for particular uses contained in The Land Use Bylaw (e.g., R-1).
<b>Low Density Residential</b>	Single family, duplex/semi-detached and low density townhouses (RR-1, R-1, RS-1, RS-2, R1-A, R-2, R2-A).
<b>Medium Density Residential</b>	Includes triplex, fourplex, townhouse and apartments to a maximum of four storeys (see RM-2, RM-4 and RM-5).
<b>Municipal Development Plan (MDP)</b>	The senior strategic planning document guiding growth and development in Calgary. It has policies relating to transportation, housing, economic activity, recreation, environmental and social issues. It also provides the strategic framework for more detailed and specific plans, policies and programs.
<b>Municipal Government Act, Part 17, Planning and Development (MGA)</b>	The provincial legislation that set out the procedures, types of arguments that can (and cannot) be considered on planning decisions and the rules that govern various planning processes. Replaced the Planning Act in 1995.
<b>Net Floor Area</b>	A total floor area, above grade, measured from the outside of the walls excluding stairways, elevators, mechanical rooms, hallways, lobbies, washrooms, garbage storage and internal parking areas.
<b>Non-conforming Buildings</b>	<p>A building:</p> <ul style="list-style-type: none"> <li>a) that is lawfully constructed or lawfully under construction at the date the Land Use Bylaw or any amendment thereof affecting the building or land on which the building is situated becomes effective, and</li> <li>b) that on the date the Land Use Bylaw or any amendment thereof becomes effective does not, or in the case of a building under construction will not, comply with the Land Use Bylaw.</li> </ul>
<b>Non-conforming Use</b>	A use that does not meet the current rules of the Land Use District for that site, but met the rules when the use commenced.
<b>Off-site Parking</b>	See Alternate Parking.
<b>Parking Area</b>	A portion of land or of a building set aside for the parking and manoeuvring of motor vehicles.

<b>Parking Standard</b>	The number of parking stalls required for different uses in different land use districts.
<b>Pathways</b>	Off-street facilities that are either shared by pedestrians and cyclists or have twinned portions which segregate the two user groups. These pathways lie in Calgary's open spaces and minimize interface with automobiles while providing a facility suitable for recreational and utilitarian use.
<b>Pedestrian-oriented or Pedestrian-friendly</b>	An environment designed to make movement (on foot or by wheelchair) fast, attractive and comfortable for various ages and abilities (e.g. visual and hearing impaired, mobility impaired, developmentally challenged). Considerations include separation of pedestrian and auto circulation, street furniture, clear directional and informational signage, safety, visibility, shade, lighting, surface materials, trees, sidewalk width, prevailing wind direction, intersection treatments, curb cuts, ramps, landscaping, etc.
<b>Pedestrian Oriented Signage</b>	Signage that is orientated to pedestrians rather than passing traffic by way of position, scale and design.
<b>Permitted Use</b>	Uses that are well-suited to a particular land use district. Applications relating to permitted uses that fully comply with the Land Use Bylaw must be approved.
<b>Perspective</b>	An illustration showing the view from a particular location as the human eye would see it.
<b>Planning Act</b>	The former provincial legislation (prior to 1995) that set out the procedures, types of arguments that can (and cannot) be considered on planning decisions and the rules that govern various planning processes. Replaced by the Municipal Government Act, Part 17, Planning & Development.
<b>Principal Building</b>	A building that accommodates the principal use of a site, and may accommodate one or more accessory uses.
<b>Principal Use</b>	The main purpose for which a building or site is used.
<b>Property Line</b>	A legal boundary of an area of land.
<b>R-1</b>	Residential Single Detached District, the purpose of which is to provide for residential development in the form of single detached housing (see <i>The City of Calgary Land Use Bylaw 2P80</i> , Section 22).
<b>R-2</b>	Residential Low Density District, the purpose of which is to provide for low density residential development in the form of single detached and duplex dwelling (see <i>The Land Use Bylaw 2P80</i> , Section 23).

<b>R-2A</b>	Residential Low Density District, the purpose of which is to provide the option of townhouse development at similar densities to single detached, semi-detached and duplex development in order to increase the variety of dwelling types (see The Land Use Bylaw 2P80, Section 24).
<b>Relaxation</b>	The term used when a change of one of the rules of the Land Use Bylaw is being considered (e.g., a residential side yard smaller than four feet).
<b>Right-of-Way</b>	A strip of land occupied or intended to be occupied by a street, crosswalk, railroad, electric transmission line, oil or gas pipeline, water main, sanitary or storm sewer main, shade trees, or other special use.
<b>Road Right-of-Way</b>	The land used for the roadway, including the sidewalk and boulevard.
<b>Rules</b>	The requirements (standards) of the Land Use Bylaw that describe such things as height maximums, side yard minimums, etc.
<b>RM-4</b>	Residential Medium Density Multi-Dwelling Districts, the purpose of which is to provide for a variety of residential dwellings in low profile form in a medium density range (see The Land Use Bylaw 2P80, Section 29).
<b>Scale</b>	Refers to the relative proportion of a structure.
<b>Section</b>	A drawing showing a vertical slice through a building or a piece of land that is a mile by a mile in size.
<b>Semi-detached Dwelling</b>	A single building that has two side-by-side dwelling units, separated from each other by a party-wall. This is compared with a duplex, which has two units, one above the other.
<b>Sensitive Intensification</b>	New development or infill development that conforms to the predominant elements of the existing streetscape.
<b>Sidewalk</b>	The area principally used for pedestrians and located to the side of a carriageway within a road right-of-way.
<b>Single-detached Dwelling</b>	A residential building that has one dwelling unit only (not including a mobile home).
<b>Site</b>	An area of land on which a building or use exists for which an application for a development permit is made.

<b>Slope Adaptive</b>	On land with significant slope ( $\geq 15\%/8.5^\circ$ ) building form should follow the slope not result in the recontouring of the land. Page 19 provides a visual illustration of how this may be achieved.
<b>Soft Landscaping</b>	Landscaping schemes that predominantly use organic elements such as trees, shrubs, flowers and lawn. The use of hard materials such as rocks and gravel are minimal.
<b>Stacked Townhouse</b>	A single building comprised of five or more dwelling units and constructed such that one or more dwelling units are located totally or partially above another, and each dwelling unit has a separated, direct entrance from grade or a landscaped area.
<b>Storey</b>	The space between the top of any floor and the top of the next floor above it, and if there is no floor above it, the portion between the top of the floor and the ceiling above it.
<b>Street</b>	A public thoroughfare, including sidewalks and borders, which affords a means of access to land abutting it and includes a lane and a bridge.
<b>Streetscape</b>	All the elements that make up the physical environment of a street and define its character including the road, boulevard, sidewalk, building setback, height and style. It also includes pavement treatment, trees, lighting, pedestrian amenities, street furniture, etc.
<b>Third Party Advertising Sign</b>	Usually a billboard.
<b>Townhouse</b>	A single building comprised of three or more dwelling units separated from one another by party-walls extending from foundation to roof, with each dwelling unit having a separate, direct entrance from grade and includes all row, linked, patio, garden court or other housing which meet such criteria.
<b>Upzoning</b>	A land use amendment that increases the allowed density or intensity of use (e.g., R-2 to RM-4).
<b>Utilities</b>	Facilities for gas, electricity, telephone, cable television, water, storm or sanitary sewer.
<b>Utility Right-of-Way</b>	Land that is used for utilities. These right-of-ways are usually shown on the legal plan and registered on the title.
<b>Walkway, Pedestrian</b>	Principally a public linkage for pedestrians only; a right-of-way or easement.

<b>Yard, Front</b>	The area extending the full width of a site and from the front property line of the site to the nearest building. Its depth is measured at right angles to the front property line.
<b>Yard, Rear</b>	That area extending the full width of a site and from the rear property line of the site to the rear of the principal building. Its depth is measured at right angles to the rear of the property line.
<b>Yard, Side</b>	That portion of the site extending from the front yard to the rear yard and between the side property line of the site and the closest side of the principal building. Its width is measured at right angles to the side property line.
<b>Zoning</b>	See Land Use Designation.

